



# Merrylands Neil Street Precinct

Development Controls





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# 1 Introduction

At its meeting on 20 October 2015, Council resolved to place the proposed amendments to Part M of the Holroyd Development Control Plan (DCP) 2013 on public exhibition. These development controls have been prepared following a Gateway Determination as received from the NSW Department of Planning and Environment (DP&E) in respect of the planning proposal to rezone and amend the maximum height of buildings and maximum floor space ratio controls of land in the Neil Street Precinct, Merrylands.

The proposed amendments to the development controls are the result of an urban design review undertaken for the Neil Street Precinct, including refinements to the Structure Plan to reflect pre Development Application discussions and approved developments within the Neil Street Precinct. The amendments to the development controls are also undertaken to support the amendments to the Holroyd Local Environmental Plan (LEP) 2013.

These development controls provide a framework to guide future development in the Neil Street Precinct. The document specifies the built form controls for all development within the Precinct, and sets in place urban design guidelines to achieve the vision for Neil Street Precinct as stated under Section 2.2.

## 1.1 Land to which these Controls Apply

These development controls apply to Merrylands Neil Street Precinct, shown edged in heavy black on Figure 1.

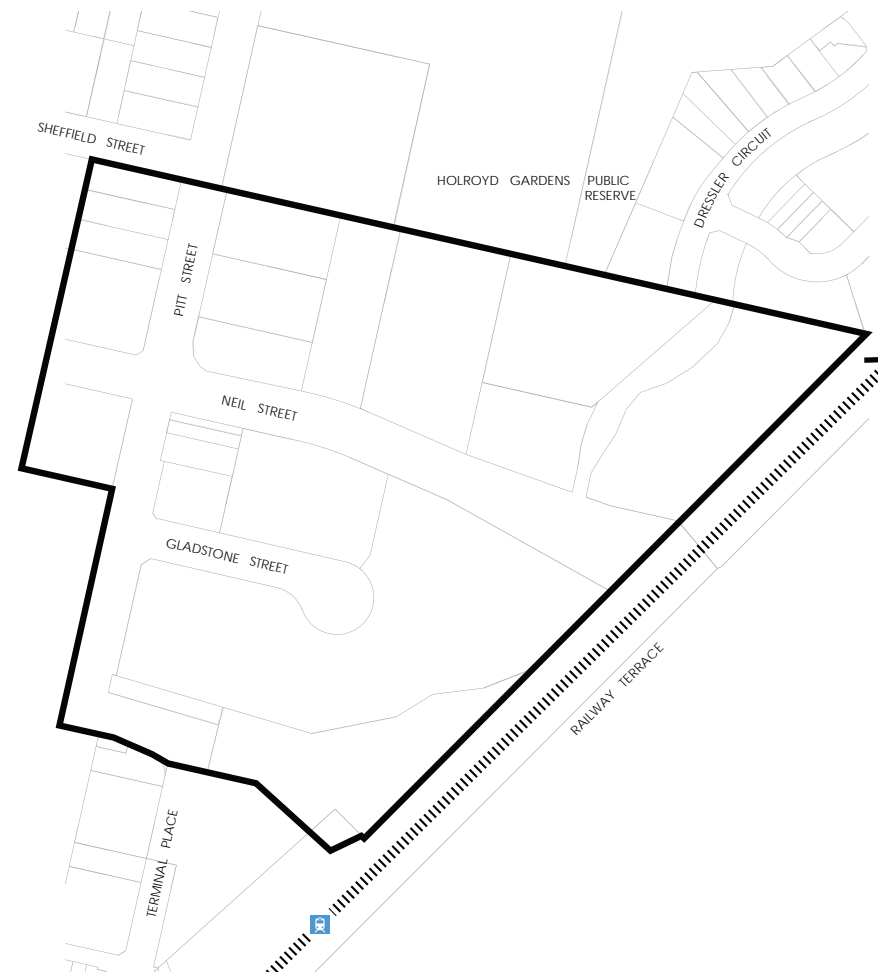


Figure 1 - Merrylands Neil Street Precinct Boundary

## 1.2 Relationships to other Instruments

Holroyd Development Control Plan (DCP) 2013 is amended by inserting these controls in Part M - Merrylands Centre Controls.

These development controls will replace all controls relevant to only the Neil Street Precinct within DCP 2013 - Part M.

## 1.3 Merrylands Neil Street Precinct

In the event of any inconsistencies between these controls and any other provisions of the DCP, the provisions in this section prevail in so far as the extent of the inconsistency.



# 2 Merrylands Neil Street Precinct Strategy

## 2.1 Aims and Purpose

The purpose of this Part is to provide objectives, controls and design criteria to achieve desirable development outcomes consistent with Council's vision for the Neil Street Precinct (the Precinct). This Part also includes Block specific objectives where applicable for the developments sites identified within the Precinct. Where objectives are not specified for a Block, the overall objectives for the Precinct should be followed.

### The aims of this Part are:-

1. To ensure the Precinct will be characterised by a high-quality, well-designed and safe environment.
2. To create an urban structure that will:
  - a. Promote a balance of residential and commercial uses within the Precinct.
  - b. Provide a transition from the more intense development near the Train Station to peripheral areas along the Holroyd Gardens.
3. To create an access network that will:
  - a. Provide a safe and convenient pedestrian environment that will encourage social interaction and encourage public transport use.
  - b. Promote greater connectivity and integration between land uses and the Train Station.
  - c. Create additional Streets that will:
    - i. Reduce pressure on Pitt Street.
    - ii. Provide new opportunities for business .
4. To create an open space network that will:
  - a. Include a network of diverse active and passive recreational spaces to support the residential and working population of the Precinct.
  - b. Provide safe, accessible, sustainable, well-used and designed open-space network.

## 2.2 Vision

The Precinct is envisioned to be characterised by a high-quality, well designed, safe and liveable environment within walking distance to Merrylands Railway Station, which is the main transport hub for the area. Properties along Pitt Street and the future development along New Road 1 will support a mix of retail, commercial office/business and residential functions.

### Urban design objectives are:-

- To enhance connectivity within the Neil Street Precinct and with the surrounds.
- To maintain and develop spaces that encourage social interaction for all people, which will contribute to people's sense of place.
- Integrate the management of stormwater and floodwater into the design of public open space to establish an adaptable public domain capable of accommodating a broad range of uses, experiences and activities, while still maintaining its primary function of overland stormwater drainage.
- To promote the "green and leafy" character associated with established trees within Holroyd Gardens.
- To maintain the sense of spaciousness created by the lower density built form and Holroyd Gardens to the north through the extensive network of private and public open space areas.
- To provide appropriate interfaces to surrounding residential and open space areas.
- To improve the visual quality of the Sydney Water Concrete Culvert by incorporating landscaping to soften the appearance which will not only provide a sustainable drainage system but also enhance the recreational value of the Precinct.



# 3 The Structure Plan

## 3.1 Desired Future Character

### Neil Street Precinct Character Statement

Neil Street is characterised by accessibility to the Holroyd Gardens to the north, Merrylands Centre to the west, Merrylands Train Station to the south, the Neil Street Park and the overland flow path recreational open space. The accessibility of Precinct is enhanced by the proposed extension of Sheffield Street to the north, proposed New Road 1 and New Road 2 and the various potential mid-block connections creating a high level of pedestrian permeability away from the main streets. Pitt Street, which is a regional road, is a significant link between Merrylands and Parramatta. Given the street hierarchy of Pitt Street, it provides an opportunity for it be established as a built form spine with ground level activity to be focused along Pitt Street.

Neil Street is the only entry from the west for the Precinct. Given the street hierarchy, Neil Street provides an opportunity for it to be established as a secondary built form spine where taller buildings can be located.

In addition to residential uses, the Neil Street Precinct is expected to accommodate commercial/retail uses that support and enhance the liveability of the place. Active uses will be located facing Pitt Street, and New Road 1 enhancing the vibrancy of the public domain.

The visual character of certain locations within the Precinct such as the intersection of Neil Street and New Road 1, the intersection of Pitt and Neil Streets, the intersection of Neil Street and the Neil Street Bridge are significant as they provide opportunities to position locational buildings, which will enhance the skyline of the Precinct within the broader Merrylands Centre context (Refer Figure 2).



Figure 2 - Neil Street Precinct Vision



# 3 The Structure Plan

## 3.2 Urban Structure Plan

The Structure Plan sets out the broad framework for development within the Neil Street Precinct. It underpins the development controls for the Precinct.

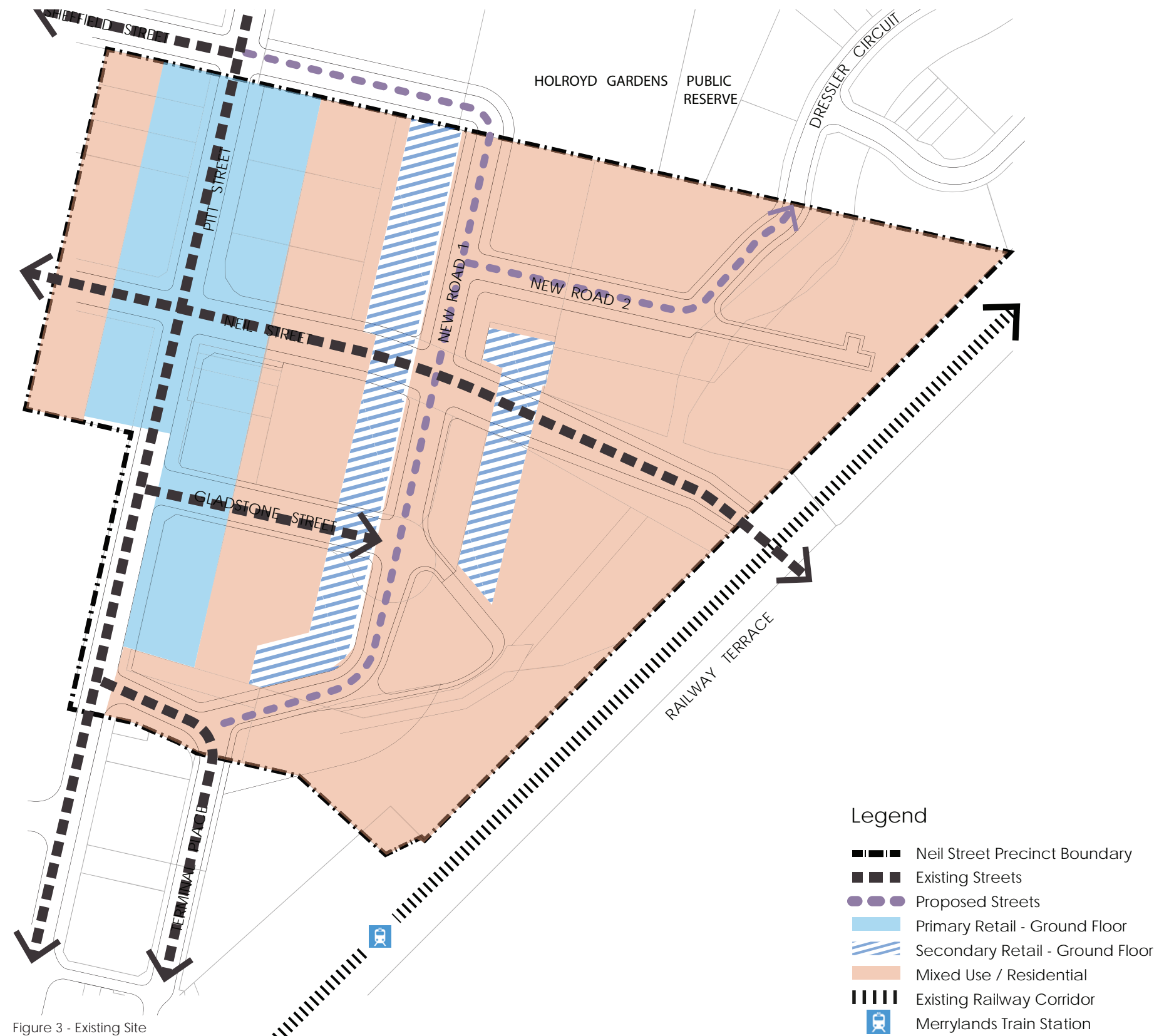
The Urban Structure Plan reflects and builds on the existing land uses and functions within the broader Merrylands Centre to implement the vision for Neil Street as a high-quality, well designed, safe and liveable environment (Refer Figure 3).

The Neil Street Precinct will predominantly include new residential communities while the commercial core will be centred on McFarlane Street and Merrylands Road (Merrylands Centre). Intense development centred within the Merrylands Centre is proposed to transition through Neil Street Precinct to the lower scaled residential areas adjoining the Neil Street Precinct.

The Structure Plan is comprised of three elements:-

- Access Network
- Public Open Space
- Built Form Network

Development in the Neil Street Precinct must occur within the framework of the Structure Plan objectives and controls, which establishes built form, open spaces and street layout. The synthesis of these elements will strengthen the desired character and connection within the broader area and create the public domain environment within which development can occur.





# 4 Access Network

## 4.1 Street / Access Network

The Street Network Structure Plan provides a clear hierarchy of street types, including the extension of existing streets and new streets. The street network is made up of the following new streets:

- New Road 1
- New Road 2

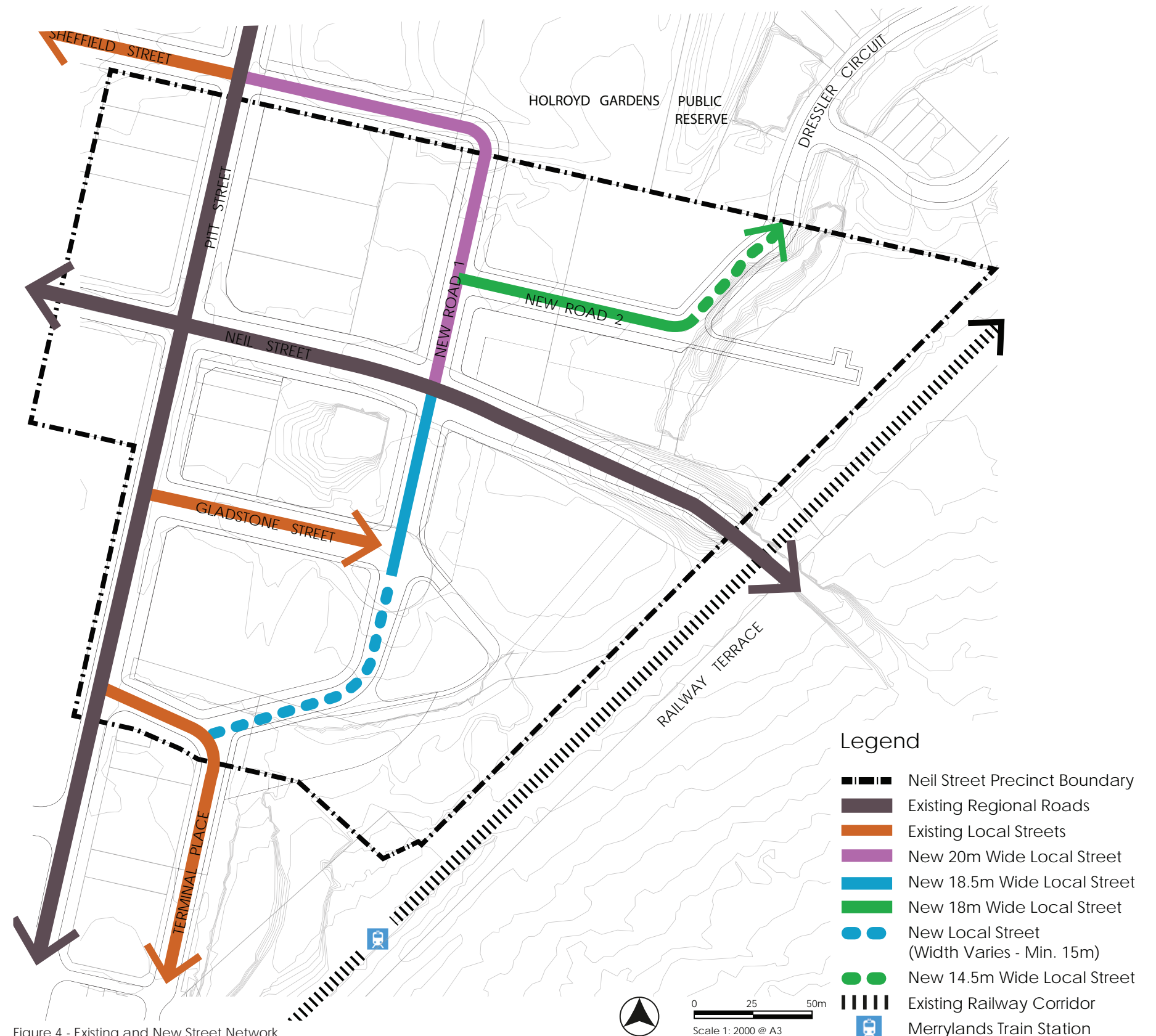
The new roads maximise connections within the Precinct and to surrounding areas and aims to substantially improve pedestrian and cycle paths to enable a more permeable public domain.

### Objectives

1. To improve pedestrian, cycle and vehicular accessibility within the Precinct and the broader Merrylands Centre.
2. To provide a street network that responds to the constraints of drainage, existing development and future subdivision patterns.
3. To provide improved access to public open spaces within the Precinct.
4. To accommodate increased traffic movement within the Precinct and broader Merrylands Centre.
5. To provide additional opportunities for on-street parking.

### Controls

- a. Provide new public streets as shown in Figure 4.
- b. Refer to Section 4.3 for detailed information regarding the required width, design and location of each street type.
- c. Setbacks along streets are to be provided in accordance with Section 8.
- d. The width of footpaths shall be maximised for comfortable pedestrian movement; to facilitate tree planting and where bike routes exist, to allow cycling off road.
- e. Streets are to be planted with trees appropriate in character to reflect the street hierarchy and in consultation with Council's landscape architect.
- f. New streets are to be dedicated to the Council. New streets are to be maintained by the landowner until dedicated to Council.
- g. Land owners within the Precinct to consult Council's engineers for detail infrastructure works.





# 4 Access Network

## 4.2 Connectivity

Through site links, arcades, visual and pedestrian/cycle connections have been established to enhance the connectivity and permeability of the Precinct and include the following (Refer Figure 5):

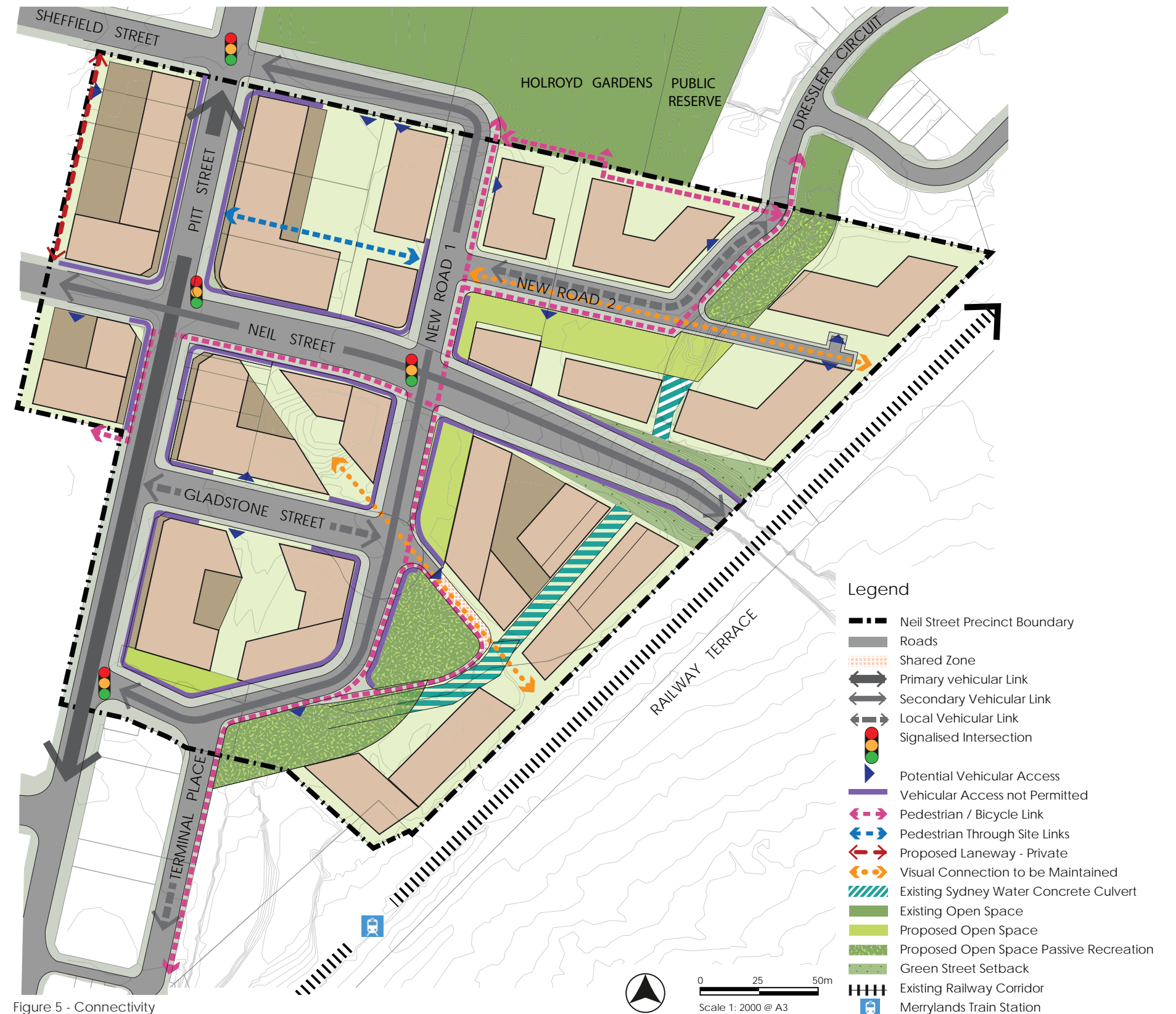
- A new pedestrian link along the northern boundary of the Precinct providing a direct link between Dressler Circuit, Holroyd Gardens and Sheffield Street.
- An east-west visual connection from New Road 1 to the Railway Corridor to the east.
- An arcade (pedestrian through site link) linking New Road 1 to Pitt Street.
- A through site link is proposed as an extension of New Road 2 to the west linking New Road 1 to Pitt Street.
- A north-south through site link connecting Neil Street to Sheffield Street at the Precinct boundary.

### Objectives

1. To ensure pedestrian ways, through-site links and arcades are accessible, continuous, well lit, safe and supported by active retail uses.
2. To encourage development that expands and enhances the Merrylands Centre public domain.
3. To promote pedestrian activity and contribute to the vitality of the Precinct.

### Controls:

- a. Provide through-site links and pedestrian ways as indicated in Figure 5.
- b. Through-site connection and arcade must:
  - i. Provide a clear sight-line from one end to the other for surveillance and accessibility, in mid-block locations.
  - ii. Have a minimum width of 12m.
  - iii. Extend and enhance the public domain and have a public domain character.



# 4 Access Network

- iv. Be designed to consider pedestrian safety and the security of adjacent businesses, particularly at night.
- c. Public use of through-site connections should be available at least between the hours of 7.00am to 7.00pm daily.
- d. Connections through foyers and shops are encouraged.
- e. Consider supplementary arcades and through-site connections, with outdoor areas such as courtyards or outdoor rooms.
- f. Vehicular entry points are not permitted along Pitt Street, Neil Street and New Road 1 south of New Road 2.

## 4.3 Streets

### 4.3.1 New Road 1

The width of New Road 1 varies between 15m at the Terminal Place intersection to 20m at the Holroyd Gardens interface. These widths are based on the predominant use and the intensity of the existing patterns of access, circulation and movement within the Merrylands Centre and the particular topographic conditions across the Precinct.

New Road 1 is intended to ease the traffic pressure from Pitt Street. To achieve greater amenity for pedestrian and cyclist movement in the public domain:

#### Controls:

- a. Buildings are not permitted to be located on any proposed street and are required to be setback from streets (Refer Section 8 for street setbacks).
- b. Lighting, paving, street furniture, landscaped setbacks and tree planting are to be provided following consultation with councils landscape officers.
- c. New Road 1 is to be provided in accordance with Figures 4, 7, 8 and 9.

### 4.3.2 New Road 2

The width of New Road 1 varies between 18m at the intersection of New Road 1 to 14.5m at the Holroyd Gardens interface.

#### Controls:

- a. New Road 2 is to be provided in accordance with Figures 4, 10 and 11.

### 4.3.3 Neil Street and Pitt Street

#### Controls:

- a. A 3m x 3m splay corner to be provided at the corner of Neil and Pitt Streets (Affected lot - 185 Pitt Street)
- b. A 0.65 road widening to be provided along Pitt Street at 185 Pitt Street to incorporate a cycle path.

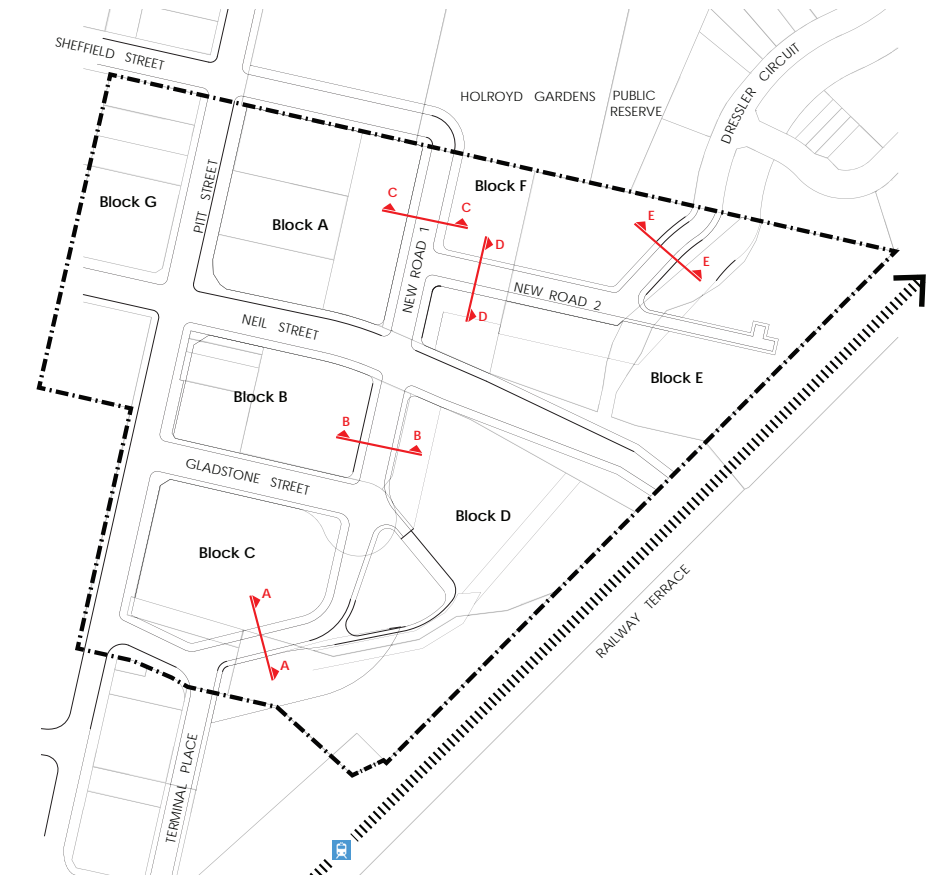


Figure 6 - Section Locations



# 4 Access Network

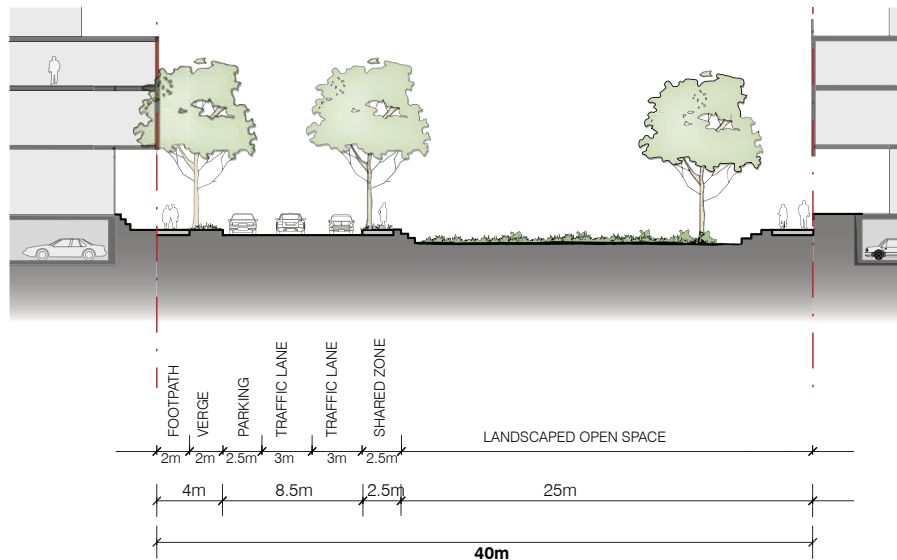


Figure 7 - Section AA

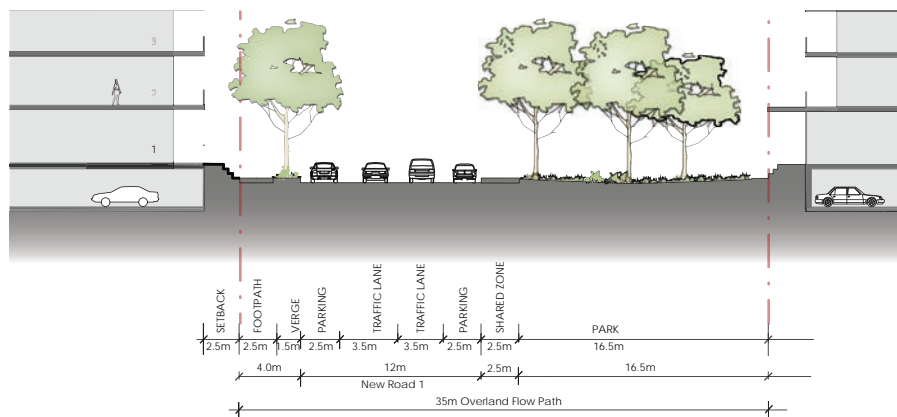


Figure 8 - Section BB

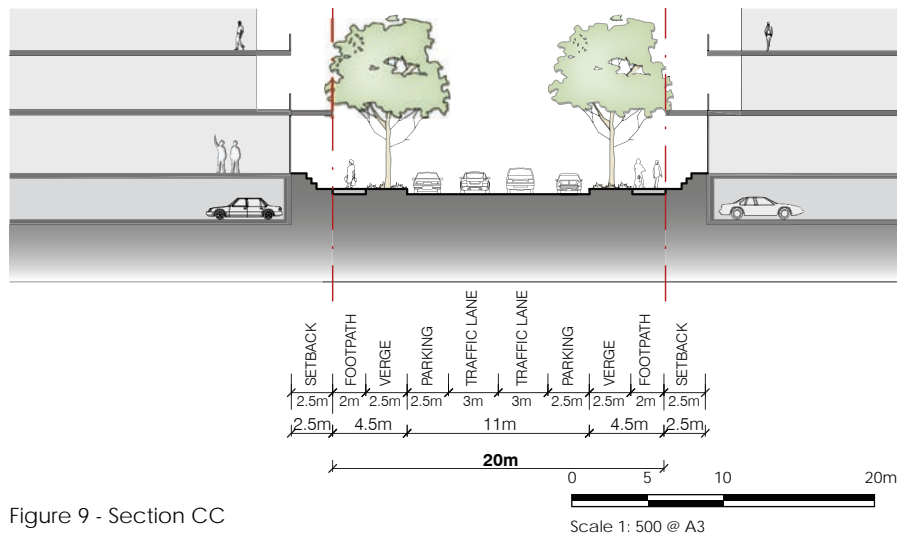


Figure 9 - Section CC

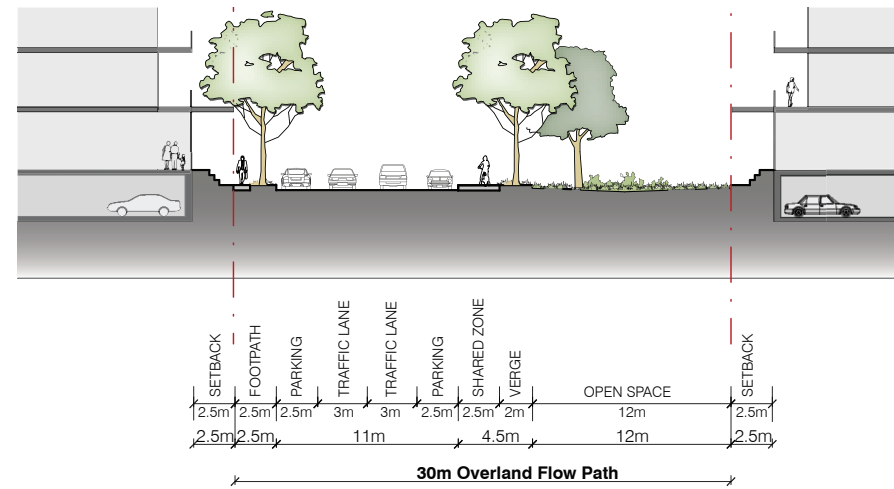


Figure 10 - Section DD

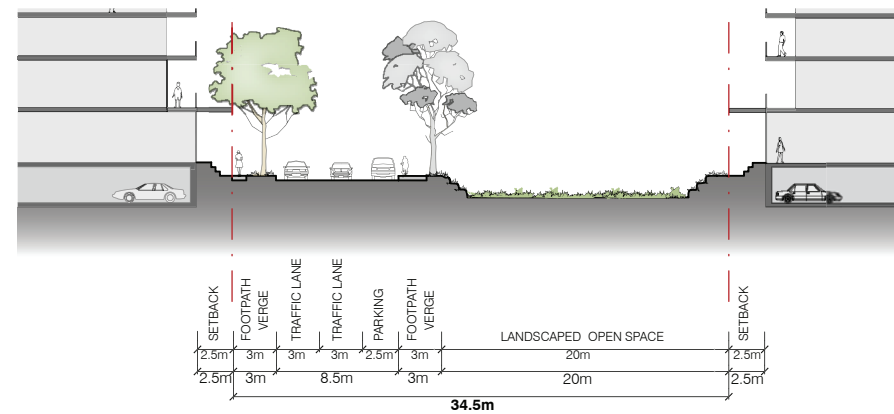


Figure 11 - Section EE



Figure 12 - Pennsylvania Avenue Washington DC - Desirable active street frontage



Figure 13 - Lonsdale Street, Dandenong - Pedestrian amenity along New Road 2  
(Source: au.pinterest.com)



Figure 14 - Street Design Ottawa - Desirable shared zone - New Road 2  
(Source: au.pinterest.com)



# 5 PUBLIC OPEN SPACE

## 5.1 Open Space Network

The Public Open Space Structure Plan creates a new open space network that will enhance the aesthetic and environmental quality of the Precinct (Refer Figure 15). The open space network contributes to the pedestrian and cycle connections, addresses water quality and overland flow and provides informal gathering and recreational space.

### Objectives:

1. To provide additional open space within a network of well connected parks and green streets.
2. To provide consolidated open spaces and open space corridors.
3. To accommodate a range of active and passive recreational uses.
4. To contribute to stormwater and ecological management.
5. To maximise the accessibility of public open space, and contribute to the pedestrian and cycle network.
6. To provide appropriate amenity, solar access and shelter across a range of uses.

### Landscape Design

- a. Public open space is to contribute to the development of a continuous canopy of native vegetation to encourage native fauna habitat.
- b. Public open space is to provide for deep soil planting, and shall have no carparking or access underneath.
- c. Public open spaces should have clear pedestrian movement routes, seating and zones of activities that are clearly defined and encourage use.
- d. With the exception of Neil Street Park and pathways, the character of the public open space shall primarily be a soft-landscaped area.
- e. The design, including paving material and furniture generally should be consistent with adjacent footpaths and/or Merrylands Centre design.
- f. Landscape design shall be compatible with the flood risk.
- g. Trees and understorey planting to comply with Crime Prevention Through Environmental Design (CPTED) principles.





# 5 PUBLIC OPEN SPACE

## Solar Access

- h. As a general rule, at least 50% of the public open space shall have access to sunlight between 9.00am and 4.00pm at the winter solstice.

## Accessibility and Connectivity

- i. Public open space is to be accessible from a variety of points within the wider public domain of Merrylands Centre.

## Diversity of Uses

- j. Buildings with zero setback to open spaces are to contain active uses for the full extent of the ground floor.

## Safety and security

- k. All public open space is to be designed to be in accordance with CPTED principles, in particular with regard to the following:
- Open sightlines and landscaping that allow high levels of public surveillance by users and residents;
  - Clear distinction between private and public open areas;
  - External lighting (in accordance with Australia Standards AS1158 - Road Lighting) which makes visible potential 'hiding spots'; and
  - Entrances to areas of public open space that encourage pedestrian use and provide visual security through the establishment of clear sightlines.

## Provisions

1. Neil Street Park
2. Terminal Place Park
3. Boulevard Park
4. Canal Greenway
5. Woodland Reserve

## 5.2 Design Criteria for Public Open Spaces

### Neil Street Park

Neil Street Park lies at the southern end of New Road 2. Its principle purpose is to serve as the major recreation space for the Precinct. It will provide a civic focus for gathering/events and work-based lunchtime breaks. It will be robust in landscape expression and largely defined by built form (Refer Figure 16).

### Objectives

1. To act as the primary soft landscaped resource for the Precinct.
2. To use the design of public domain elements and furniture, and the surface materials to create a distinctive character.
3. To be adoptable as a performance space with informal seating areas.

### Desired Character

- Activation of ground floor commercial uses along New Road 2 and development to the north.
- Create a sense of place (Refer Figure 17).

### Controls:

- a. Provide a minimum 1,500sqm public open space - Neil Street Park as shown in Figure 19.
- b. Neil Street Park is to be in public ownership.

### Terminal Place Park

Located at the southern end and along New Road 1, Terminal Place Park provides a transition space between the predominantly residential Precinct and the Merrylands Centre. The principle aim is to provide seating and shade for passive recreation in the vicinity of the train station (Refer Figure 20).

### Objectives

1. To provide additional resource to the local residents and commuters.
2. To reinforce a sense of safety for the community by providing appropriate lighting and directional signage.



Figure 16 - Pennsylvania Avenue Park, Washington DC - Public open space defined by built form  
(Source: au.pinterest.com)



Figure 17 - Artist impression Sheas Park - Green Square, Sydney. The overland flow path designed as a space for passive recreation and pedestrian link.  
(Source: www.landcom.com.au)



# 5 PUBLIC OPEN SPACE

- To provide sufficient furniture such as bins, seats, lighting and bi-cycle parking in appropriate locations.

## Desired Character

- Predominantly soft landscape with hardscape elements to accommodate seating and public art.
- Open lawn areas for passive recreation (Refer Figure 20).

## Controls:

- Provide a minimum width of 25m as shown in Figure 19.

## Boulevard Park

Boulevard Park lies along the eastern edge of New Road 2 Between Gladstone and Neil Streets.

## Objectives

- To provide a passive recreational space for surrounding development.

## Desired Character

- Activation of ground floor retail/commercial edge to the east and western edge of New Road 2.
- Design should reflect the desire line to Holroyd Gardens.

## Controls:

- Provide a minimum width of 16.5m as shown in Figure 19.

## Canal Park

Canal Park lies along the southern edge of New Road 2 and wraps around the eastern edge of New Road 1.

## Objectives

- To continue the “green link” of the Precinct and provide a leafy setting to the predominantly residential use of the area north of Neil Street.

## Desired Character

- Soft landscaping integrating where possible the Sydney Water Canal corridor.
- Ability to accommodate passive recreation.
- Planting of endemic and cultural species.

## Controls:

- Provide minimum widths as shown in Figure 18.

## Woodland Reserve

Canal Park lies along the eastern edge of New Road 2. The principle aim is to provide a connection both visual and physical, being the location for major cycle and footpath links at the local level.

## Objectives

- To provide a green link to A'Becketts Creek and the riparian corridor to the north and the new Neil Street Precinct landscape network.

## Desired Character

- Accommodate range of experiences and activities including informal walking tracks and seating (Refer Figure 25).
- Continue the natural woodland character of the existing A'Becketts Creek to the north with planting of indigenous native species.
- Low maintenance, robust plant species and finishes.

## Controls:

- Provide minimum widths as shown in Figure 18.



Figure 18 - Canal Park and Woodland Reserve

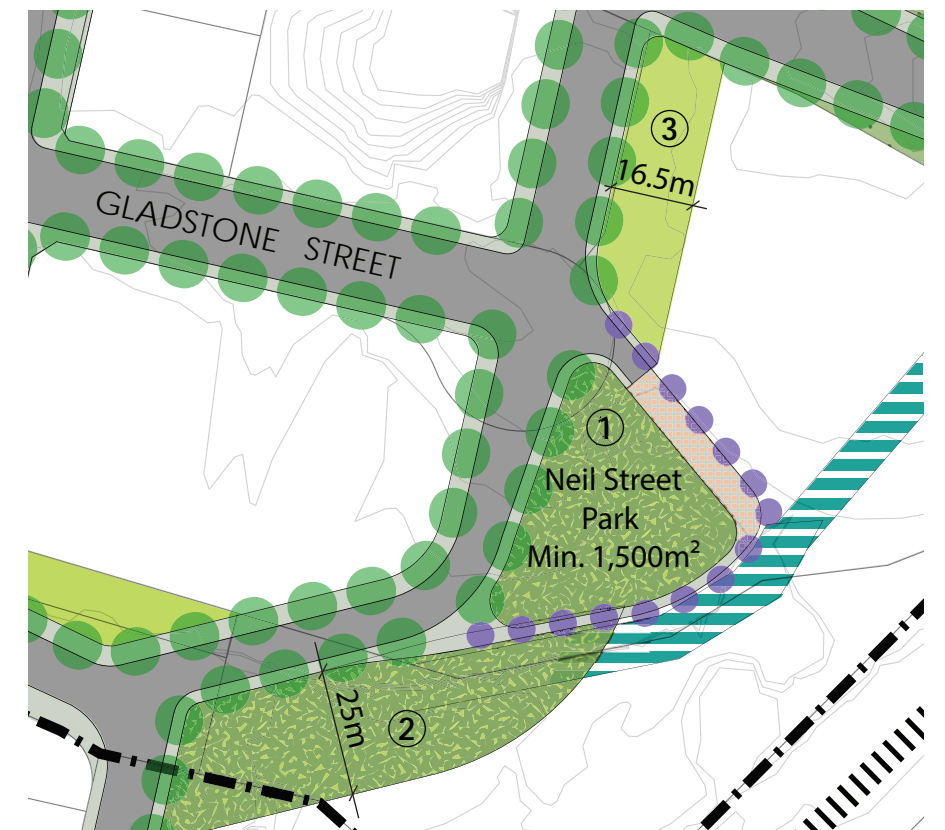


Figure 19 - Neil Street Park, Canal Park and Boulevard Park



# 5 PUBLIC OPEN SPACE



Figure 20 - Lakeshore East - Chicago: Combination of overland flow path and passive recreational space. Terminal Place Park character.  
(Source: [au.pinterest.com](http://au.pinterest.com))



Figure 21 - North Carolina Museum of Art detention basin converted to wetland  
(Source: [surface678.com/north-carolina-museum-of-art-pond-4](http://surface678.com/north-carolina-museum-of-art-pond-4))



Figure 22 - Melbourne Docklands - Overland flow path as a passive recreational space.  
(Source: [www.aecom.com.au](http://www.aecom.com.au))



Figure 23 - Bioswale amphitheatre, Manassas Park Elementary School, Virginia, USA.  
(Source: [americaninstituteofarchitects.com/top10projects](http://americaninstituteofarchitects.com/top10projects))



Figure 24 - Central Park Sheffield, UK. Boulevard Park character.  
(Source: [au.pinterest.com](http://au.pinterest.com))



Figure 25 - Woodland Park.  
(Source: [au.pinterest.com](http://au.pinterest.com))



# 6 BUILT FORM

## 6.1 Built Form Network

### Built Form Structure Plan Principles

One of the principle urban design strategies which guided the built form structure plan for the Merrylands Centre was to provide height transition from the lower scale residential buildings to the higher scale buildings on Merrylands Road and Mcfarlane Street.

The Built Form Structure Plan builds on the strategies established for the Merrylands Centre and focuses on the character and height distribution of built form within the Neil Street Precinct. This structure supports the density controls contained within LEP 2013.

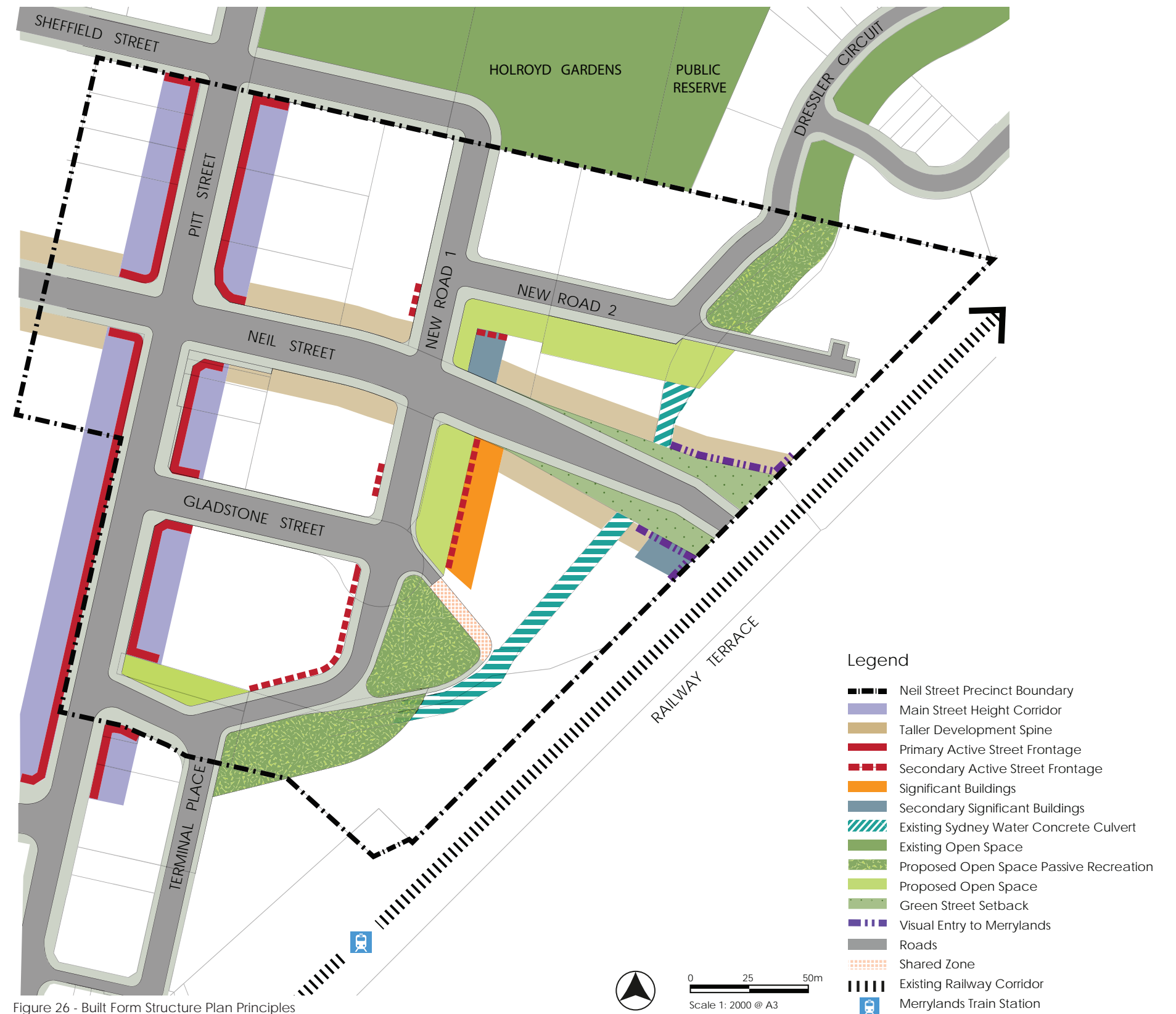
The taller built elements have been strategically arranged along major streets and adjacent to the open space network, defining the edge of the overland flow path/green corridor.

Opportunities for taller buildings have been identified. These sites spatially locate important places within the Precinct such as key entry point and parks (Refer Figure 26). The taller buildings are intended to be distinct from their lower scale surrounding and provide visual reference and urban legibility. The visual impact of the proposed increase in heights has been analysed in relation to the broader context of the Merrylands Centre.

The principal tower is located adjacent the Neil Street Park and in proximity to the Neil Street Precinct entry off the Neil Street Bridge. The secondary towers are located on the east-west development spine at significant locations.

The important street corridor of Pitt Street is reinforced by consistent height and street setback. Within the street network opportunities for active frontages have been identified and controls provided for the specific relationship between buildings and the street in these locations.

Location of active street level uses are identified adjacent to the green link.





# 6 BUILT FORM

## Built Form Structure Plan

The Built Form Structure Plan is a broad, long term plan to guide changes in built form and provide clear direction about preferred locations of buildings within developments and building separations. The building footprints indicated on Figure 27 represent the preferred building configuration. Buildings are to be designed in accordance with Section 8 – Site Specific Controls.



# 7 Site Amalgamation

Seven development sites (blocks) within the Precinct have been identified resulting from the evolving land ownership pattern and road alignment (Refer Figure 28). These blocks are anticipated to cater to the future increases in population and pedestrian movements, particularly those arriving via the rail network. In addition, these blocks incorporate significant public spaces and parks supporting the commercial and residential uses within and around the Precinct. Land uses have been coordinated with the desired built form outcomes to ensure that the Precinct functions as a highly attractive, safe and usable urban space.

## Controls

1. Amalgamation of lots in accordance with Figure 28 is desired for redevelopment.
2. Land amalgamation is to increase the width of the street frontage and avoid irregular lot configuration.
3. Sites are to be amalgamated to avoid isolating an adjoining site or sites.
4. The lot shape, orientation and design of amalgamation and subdivision lots is to support the following:
  - i. Protection and enhancement of the amenity, solar access, privacy, open space and views of the neighbouring lots
  - ii. Incorporation of the principles of water sensitive urban design
5. The block width, dimension, orientation and layout are to consider the existing subdivision pattern of the locality.
6. New lot/s created must be such that each lot with street frontage allows for the siting of a development which will address the street.



Figure 28 - Preferred Site Amalgamation



# 8 Site Specific Controls

This section provides character statements, objectives and development controls for specific areas/blocks within the Precinct as identified under Section 7. These blocks will contribute to the identity, function and character of the Precinct and as such more detailed built form controls have been provided to ensure high quality outcomes.

## General Precinct Controls

- General controls applicable to the whole Precinct are as follows:

Building Envelopes		
Maximum Horizontal Length of Buildings (above any podium)	<ul style="list-style-type: none"><li>9 to 12 Storeys = Max. 75m</li><li>13 to 20 Storeys = Max. 55m</li></ul> <p>The max. horizontal length of any building without substantial articulation shall not exceed 45m.</p>	
Building Breaks		
Buildings	Please refer to Section 8 for the location of preferred building separation requirements.	
Solar Access		
Residential Part of Buildings	<ul style="list-style-type: none"><li>Min. 2 hours direct sunlight access to 70% of apartments between 9.00am to 4.00pm at the winter solstice (22 June).</li></ul>	
Public Open Space	Neil Street Park <ul style="list-style-type: none"><li>Min. 2 hours direct sunlight between 12noon to 3.00pm at the winter solstice (22 June) to min. 50% of the area.</li></ul>	
	Other Public Open Spaces <ul style="list-style-type: none"><li>Min. 2 hours direct sunlight between 9am to 4.00pm at the winter solstice (22 June) to min. 50% of the area.</li></ul>	

Street Activation	
Pitt Street and Terminal Place	<ul style="list-style-type: none"> <li>Fully activate at least 2 storeys with commercial/retail uses.</li> </ul> <p><b>B4 Zone</b> Min. non-residential GFA equivalent to 40% of the ground floor building footprint area.</p> <p><b>B6 Zone</b> Minimum non-residential GFA equivalent to 20% of the ground floor building footprint area except for the site at the southeast corner of Neil Street and new Road 1 where the minimum requirement for street activation is 50% of the ground floor building footprint area.</p>
Western Side of New Road 1	<ul style="list-style-type: none"> <li>Area between Terminal Place and Neil Street to be intermittently activated as a secondary active frontage.</li> </ul>
Street Wall Height	
Along Pitt Street	<ul style="list-style-type: none"> <li>3 storey podium with a minimum height of 11m and maximum 14m.</li> </ul>
Parking	
Parking	<ul style="list-style-type: none"> <li>Parking must be provided in the basement (underground).</li> <li>Underground parking is not permitted to encroach into the setback areas or under public open space areas.</li> <li>Please refer to Part A - General Controls for other parking requirements.</li> </ul>
Building Envelope Depth	
Commercial / retail (Above Podium)	<ul style="list-style-type: none"> <li>Max 25m (unless specified in Section 8).</li> </ul>
Residential	<ul style="list-style-type: none"> <li>Max 22m (unless specified in Section 8).</li> </ul>
Public Domain Interface	
Vehicle Access	<ul style="list-style-type: none"> <li>Vehicle access should not ramp along boundary alignments facing a street or public open space.</li> </ul>

Awning	
Along Pitt Street & Eastern Edge of Boulevard Park	<ul style="list-style-type: none"> <li>Awnings should be provided along Pitt Street.</li> <li>Min. 3m deep.</li> <li>Preferred minimum soffit height of 3.3m.</li> <li>Slim vertical fascias/eaves not more than 300mm in height.</li> <li>Wrap awnings around corners where a building is sited on a street corner.</li> </ul>
Site and Building Design	
Unless specified under Section 7 or 8, for the design of apartments/mix use building design ; please refer to the Apartment Design Guide (ADG); NSW Government Planning & Environment for: <ul style="list-style-type: none"> <li>Building depth</li> <li>Building separation</li> <li>Deep soil zones</li> <li>Visual privacy</li> <li>Communal and public open space</li> <li>Pedestrian access and entries</li> <li>Vehicle access</li> <li>Bicycle and car parking</li> <li>Building Amenity (Ceiling height, Solar access, Natural ventilation, Private open space and balconies, Acoustic Privacy, Noise and pollution, Common circulation and spaces)</li> <li>Building Configuration (apartment mix, layout and size, storage, roof design,landscape design, planting on structures, façades, awnings)</li> <li>Performance (Energy efficiency, Waste Management, private open space and balconies, Water management and conservation).</li> </ul>	
Stormwater Management	
Merrylands Neil Street Precinct is affected by the 1 in 100 year flood. Roads and open space network have been designed as overland flow path to manage the impact of flooding. To ensure appropriate flood management: <ul style="list-style-type: none"> <li>Width and location of the overland flow path to be in accordance with Section 4 and 8.</li> <li>Please refer to Part A - General Controls.</li> <li>Consult with Council's engineers prior to submitting a DA.</li> </ul>	

## 8 Site Specific Controls

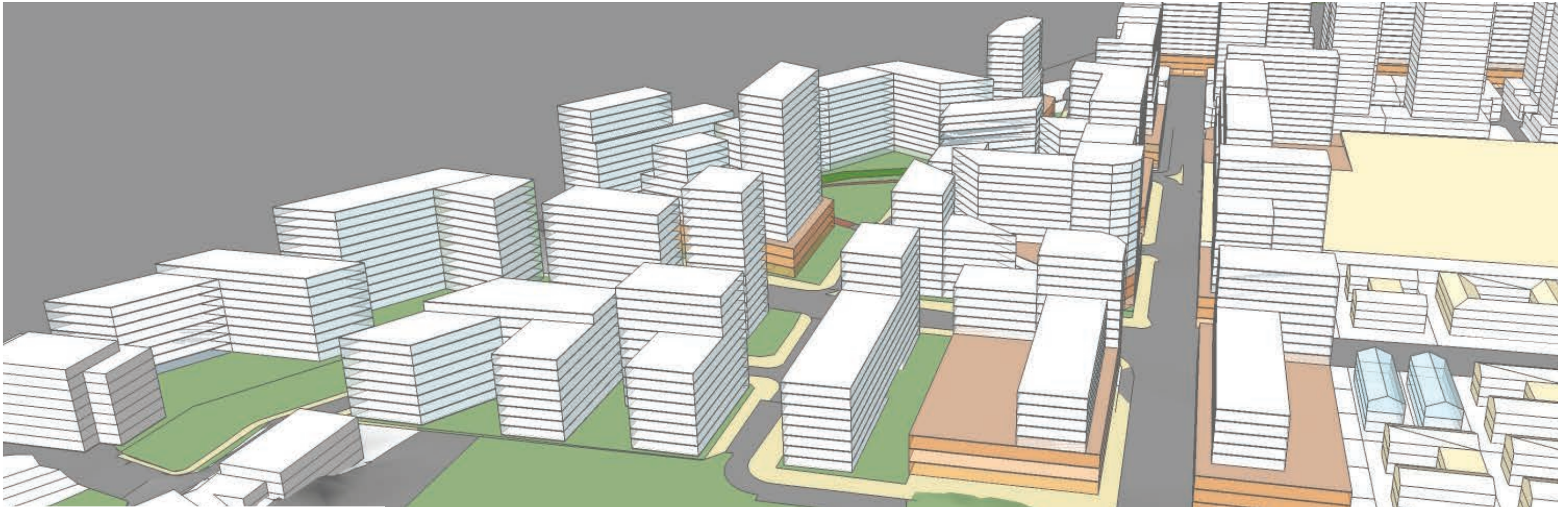


Figure 29 - Proposed Built Form Model - Looking South

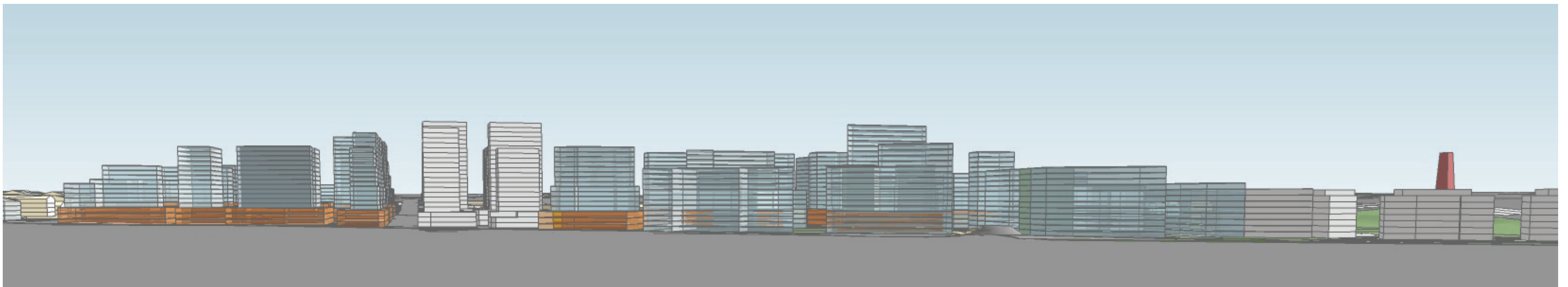


Figure 30 - Proposed Built Form Model - Looking West from the Railway Corridor



# 8 Site Specific Controls

## 8.1 Block A

Block A is bounded by the Holroyd Gardens to the north, Block F to the east, Neil Street to the south and Pitt Street to the west.

The detailed, site specific controls within this section will define the scale and character of development at the Pitt and Neil Streets intersection, providing development that creates a positive image.

The objectives are as follows:-

1. To ensure the development contributes to the provision of public infrastructure.
2. To provide a range of uses supporting the predominantly commercial use within the Merrylands Centre, and generating activity at ground level.
3. To ensure that the intersection of Pitt and Neil Streets creates a quality identity for the corner.
4. To ensure scale and form of development contributes to the public domain and legibility of Pitt Street.

### 8.1.1 Site and Building Design

#### Public Domain

The key public domain features of this Block are:

- New Road 1 to the north
- Pitt Street to the west

New street improvements are to be provided to both the streets.

#### Controls

- a. Primary active frontages are to be provided where shown in Figure 31.
- b. Primary active frontage are to have a civic character.
- c. Awnings to be provided along Pitt Street.

### 8.1.2 Building Heights

Refined building heights are provided to determine the extent and location of height distribution within the Precinct.

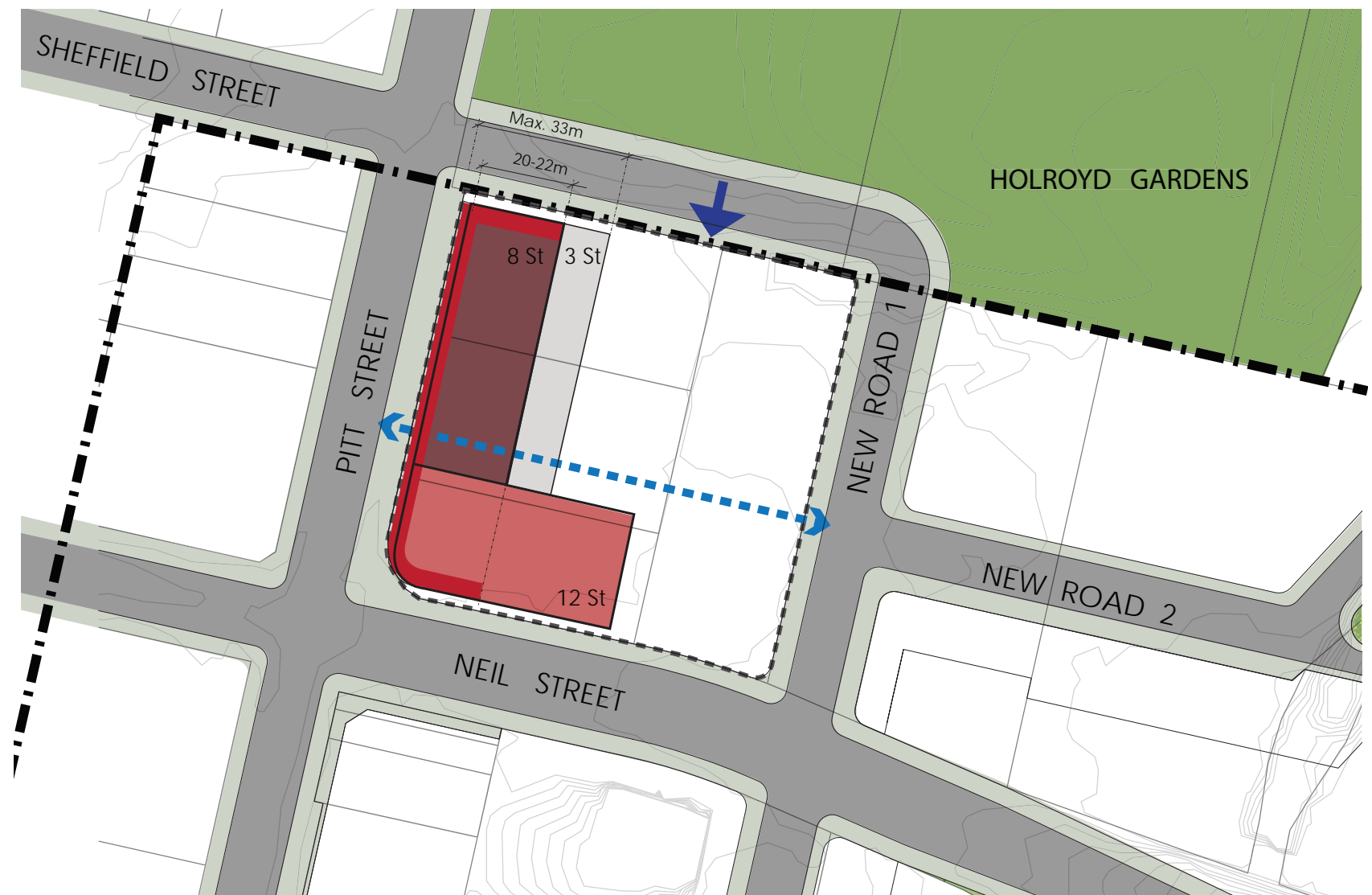


Figure 31 - Proposed Block A Height and Public Domain Plan

#### Legend

- Neil Street Precinct Boundary
- - - Block A Boundary
- Primary Active Street Frontage
- Existing Open Space
- ↔ Pedestrian Through Site Link
- ➡ Preferred Vehicular Access Point
- Roads
- 3 Storey Podium
- 30m (8 Storey B4/B6 Zone)
- 42m (12 storey - B4/B6 Zone)

# 8 Site Specific Controls

## Controls

- d. Development should comply with the Block A Height Plan which indicates the maximum number of permissible storeys (Refer Figures 31 and 32).

### 8.1.3 Setbacks

To provide some flexibility in the configuration of buildings on site, building zones have been identified within which buildings can occur on the site. The building zone is determined by the street, side and rear setbacks.

The building zone cannot be totally taken up by buildings. The extent of the building zone that can be occupied by buildings is calculated by applying all the built form controls the Precinct. The building configuration indicated in the diagrams is the preferred building configuration.

## Controls

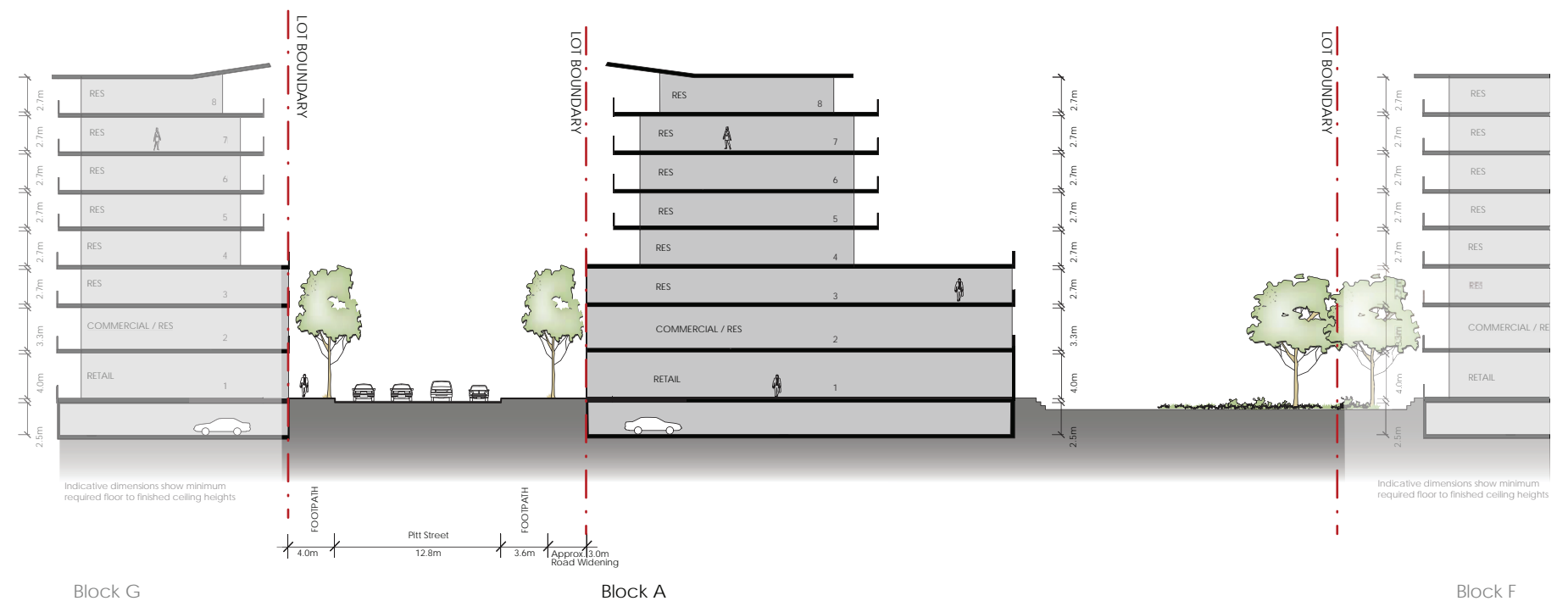
- Provide setbacks as shown in Figure 34.

### 8.1.4 Public Domain Interface

Specific street frontage treatments are required to achieve consistency within and around the Precinct, and to reinforce the desired streetscape character. The streetscape character is determined by the design and consistency of the building edge, and the continuity of the built form interface relative to driveways and vehicular crossing.

## Controls

- Driveways and vehicular crossings are not permitted along Pitt Street
- Driveways and vehicular crossings are to be provided from New Road 1. Indicative locations are shown in Figure 31.





# 8 Site Specific Controls

Building Height	
Along Pitt Street	• Max 12 storeys (Refer Figure 31)
Along Neil Street	• Max 12 storeys (Refer Figure 31)
Building Use	
B4 Zone - Along Pitt Street	Ground and first floor
	• Commercial / retail
B4 Zone - All other buildings	Second floor and above
	• Commercial or residential
B4 Zone - All other buildings	Ground floor
	• Commercial / retail
B4 Zone - All other buildings	First Floor and above
	• Residential / commercial
Building Envelope Depth	
Commercial / retail (Above Podium)	• Max 25m
Residential	• Max 22m
Setback	
Street setback	Pitt Street
	• 0m
	Neil Street
	• Min 2.5m
Rear setback	Sheffield Street Extension
	• Min 2.5m
Rear setback	For lots fronting Pitt Street
	• Min 6m and 12m
Street Wall Height	
Along Pitt Street	• 3 storey podium with minimum height of 11m and maximum 14m.
Awning	
Along Pitt Street	• Min. 3m deep

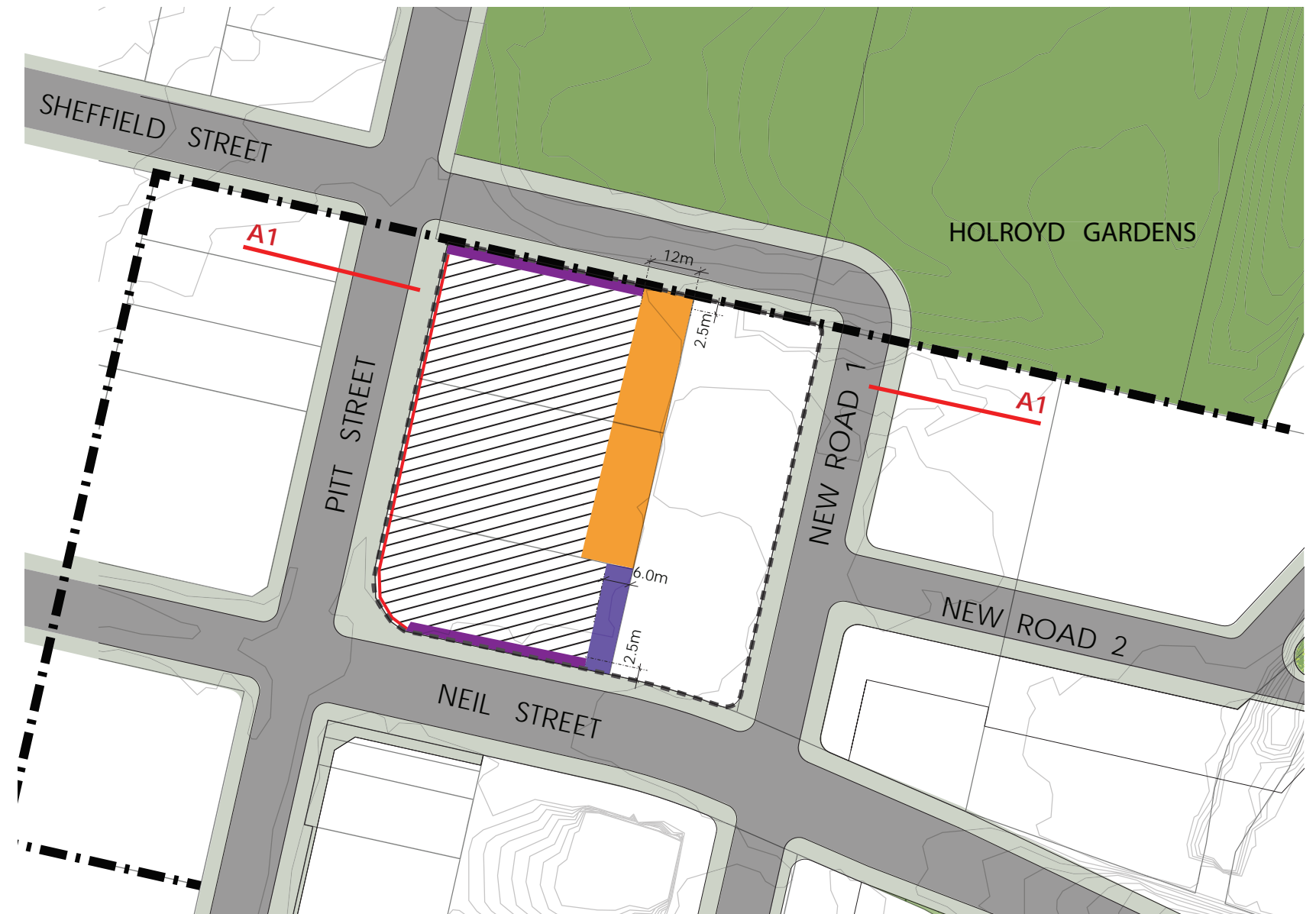


Figure 34 - Block A Setback Plan

## Legend

- Neil Street Precinct Boundary
- Block A Boundary
- Existing Open Space
- Roads
- Building Zone
- 0m Setback
- Min. 2.5m Setback
- Min. 6m Setback
- Min. 12m Setback



0 25 50m  
Scale 1: 1500 @ A3



# 8 Site Specific Controls

## 8.2 Block B

Block B is bounded by Neil Street to the north, New Road 1 to the east, Gladstone Street to the south and Pitt Street to the west.

The objectives are as follows:-

- To provide a range of uses supporting the predominantly commercial use within the Merrylands Centre, and generating activity at ground level
- To ensure scale and form of development contributes to the public domain and legibility of Pitt Street

### 8.2.1 Site and Building Design

#### Public Domain

The key public domain features of this Block are:

- Neil Street to the north
- New Road 1 to the east
- Gladstone Street to the south
- Pitt Street to the west

#### Controls

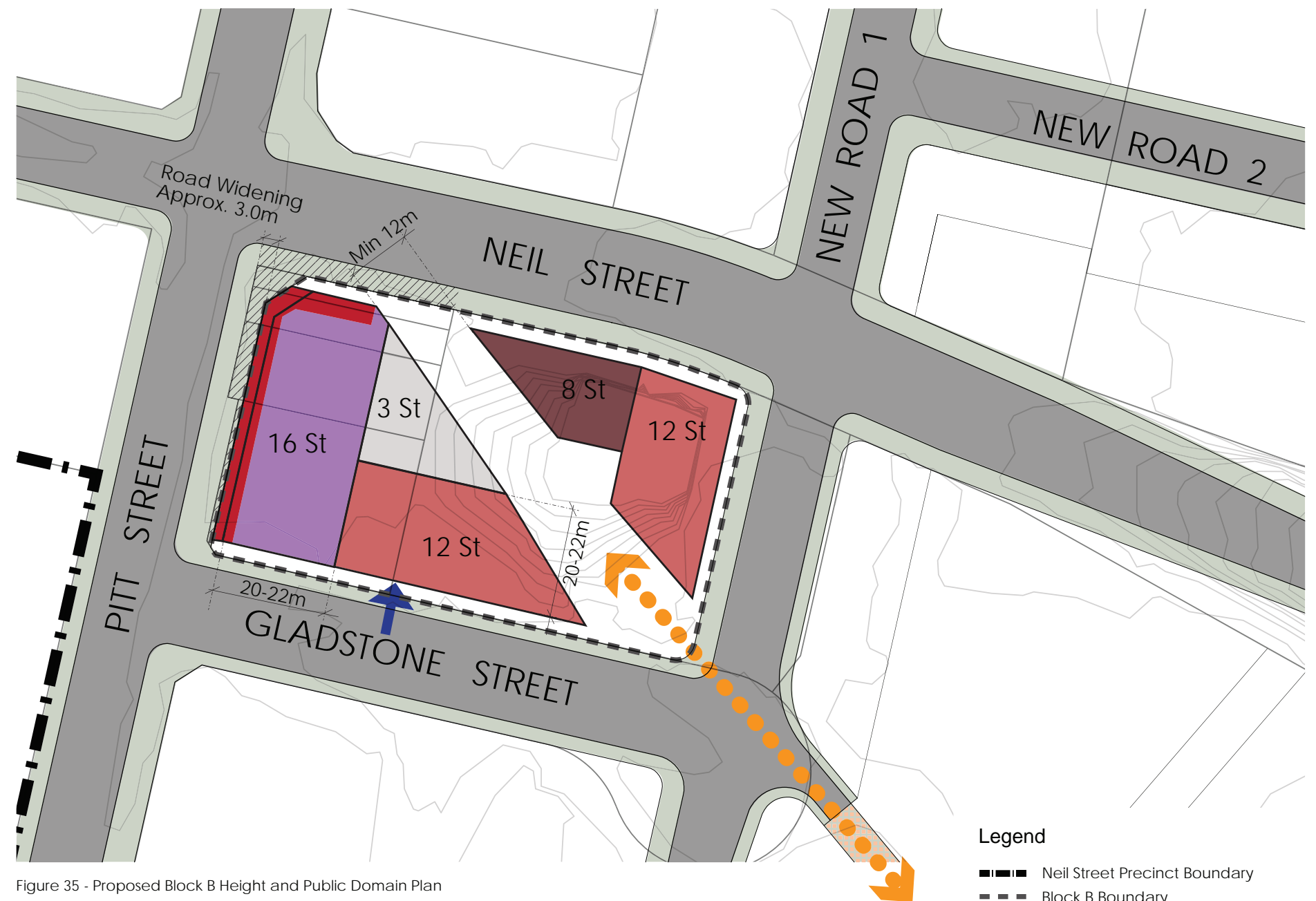
- Primary active frontages are to be provided where shown in Figure 35.
- Primary active frontages are vibrant and inviting.

### 8.2.2 Building Heights

Refined building heights are provided to determine the extent and location of height distribution within the Precinct.

#### Controls

- Development should comply with Block B Height Plan which indicates the maximum number of permissible storeys (Refer Figure 35 and 36).





# 8 Site Specific Controls

## 8.2.3 Setbacks

To provide some flexibility in the configuration of buildings, building zones have been identified within which buildings can occur on the site. The building zone is determined by the street, side and rear setbacks.

The building zone cannot be totally taken up by buildings. The extent of the building zone that can be occupied by buildings is calculated by applying all the built form controls for the Precinct. The building configuration indicated in the diagrams is the preferred building configuration.

### Controls

- Provide setbacks as shown in Figure 37.

## 8.2.4 Public Domain Interface

Specific street frontage treatments are required to achieve consistency within and around the Precinct, and to reinforce the desired streetscape character. The streetscape character is determined by the design and consistency of the building edge, and the continuity of the built form interface relative to driveways and vehicular crossings.

### Controls

- Driveways and vehicular crossings are not permitted along Pitt Street
- Driveways and vehicular crossings are to be provided from New Road 1. Indicative locations are shown in Figure 35.

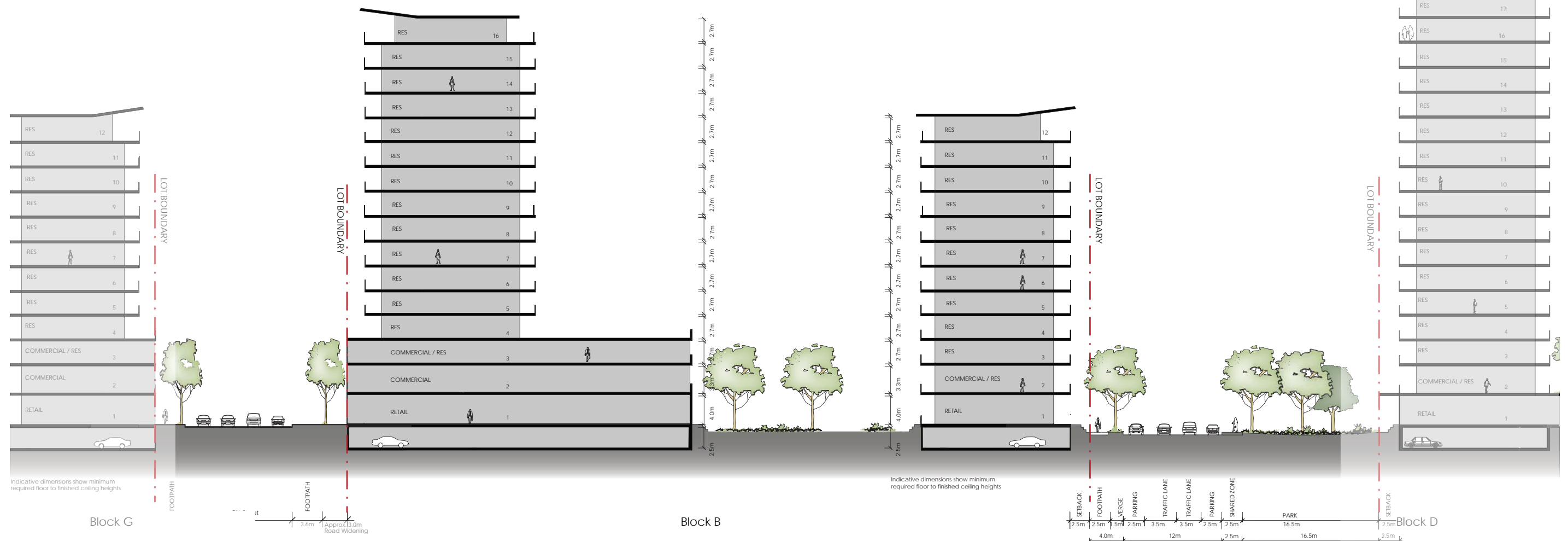


Figure 36- Section B1 - B1



# 8 Site Specific Controls

Building Height	
Along Pitt Street	• Max 16 storeys (Refer Figure 35)
Along New Road 1 and Gladstone Street	• Max 12 storeys (Refer Figure 35)
Along Neil Street	• Max 8 storeys
Building Use	
B4 Zone - Along Pitt Street	Ground and first floor • Commercial/retail Second floor and above • Commercial/retail or residential
B6 Zone - Along New Road 1	Ground floor • Commercial/retail/residential First floor and above • Residential / Commercial
B6 & B4 Zone - Along Neil Street	All floors residential
Building Envelope Depth	
Commercial/retail (Above Podium)	• Max 25m
Residential	• Max 22m
Setback	
Street setback	Pitt Street • 0m
	Neil Street, New Road 1 and Gladstone Street • Min 2.5m



Figure 37 - Proposed Block B Setback Plan

# 8 Site Specific Controls

## 8.3 Block C

Block C is bounded by Gladstone Street to the north, New Road 1 to the east, Terminal Place to the south and Pitt Street to the west. Block C has similar characteristics as Block B.

### 8.3.1 Site and Building Design

#### Public Domain

The key public domain features of this Block are:

- Gladstone Street to the north
- New Road 1 to the east
- Terminal Place to the south
- Pitt Street to the west

#### Controls

- Primary active frontages are to be provided where shown in Figure 38.
- Primary active frontages are to be vibrant and inviting.

### 8.3.2 Building Heights

Refined building heights are provided to determine the extent and location of height distribution within the Precinct.

#### Controls

- Development should comply with Block B Height Plan which indicates the maximum number of permissible storeys (Refer Figure 38).

### 8.3.3 Setbacks

To provide some flexibility in the configuration of buildings, building zones have been identified within which buildings can occur on the site. The building zone is determined by the street, side and rear setbacks.

The building zone cannot be totally taken up by buildings. The extent of the building zone that can be occupied by buildings is calculated by applying all the built form controls for the Precinct. The building configuration indicated in the diagrams is the preferred building configuration.

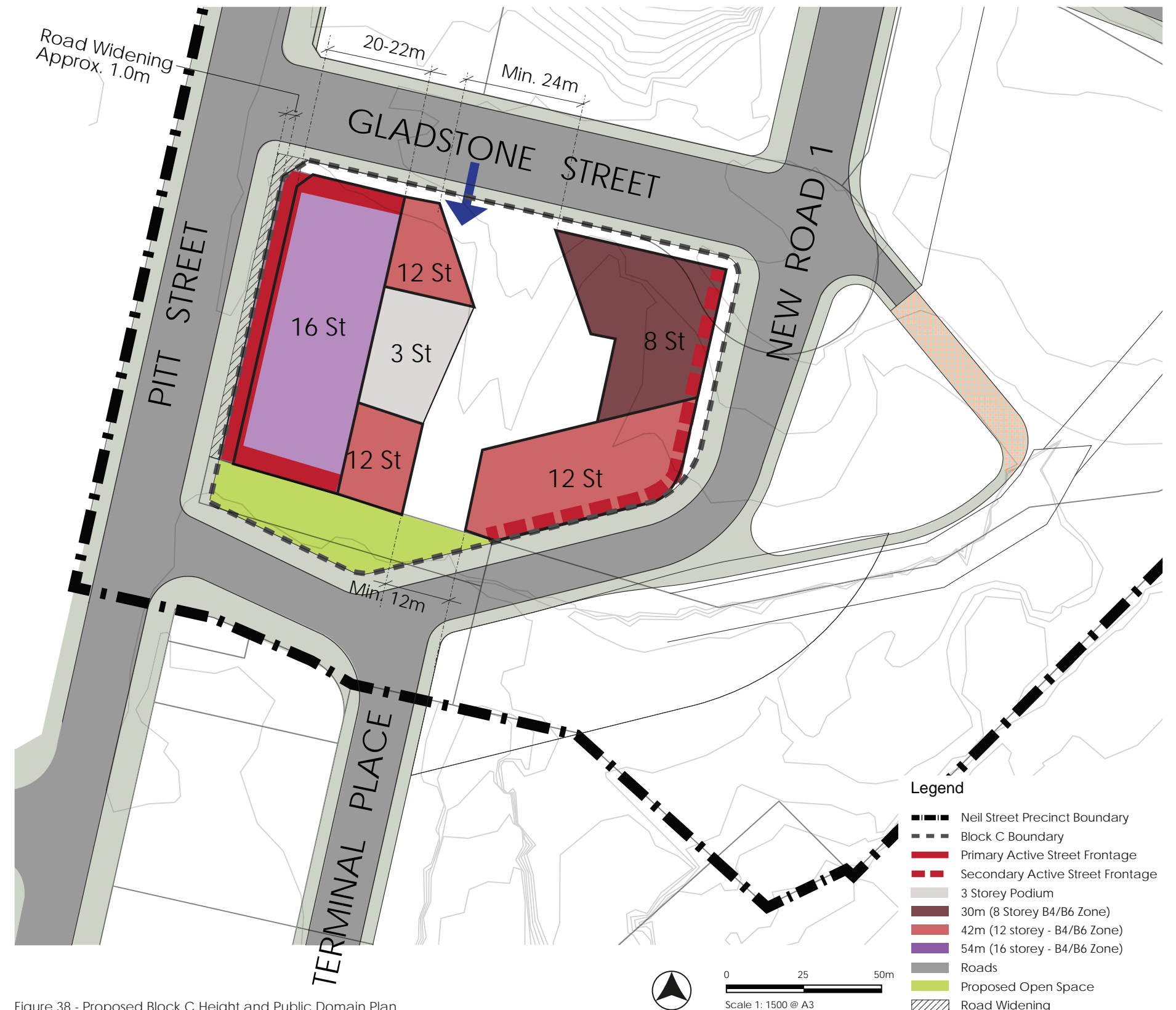


Figure 38 - Proposed Block C Height and Public Domain Plan



# 8 Site Specific Controls

## Controls

- Provide setbacks as shown in Figure 43.
- Underground parking is not permitted to encroach into the setback areas.

### 8.3.4 Public Domain Interface

Specific street frontage treatments are required to achieve consistency within and around the Precinct, and to reinforce the desired streetscape character. The streetscape character is determined by the design and consistency of the building edge, and the continuity of the built form interface relative to driveways and vehicular crossings.



Figure 39 - Artist Impression of Pinnacle Towers, South Perth - Podium and Tower Form  
(Source: [www.pinnaclesouthperth.com](http://www.pinnaclesouthperth.com))



Figure 40 - Rouse Hill, Sydney. Zero street setback with active street frontage



Figure 41 - Three Storey Commercial/Retail Podium with Active Street Frontage ((Source: [www.pinnaclesouthperth.com](http://www.pinnaclesouthperth.com))



Figure 42 - The horizontal and vertical architectural elements provide interest and break the monotony of the elevation and scale of the building

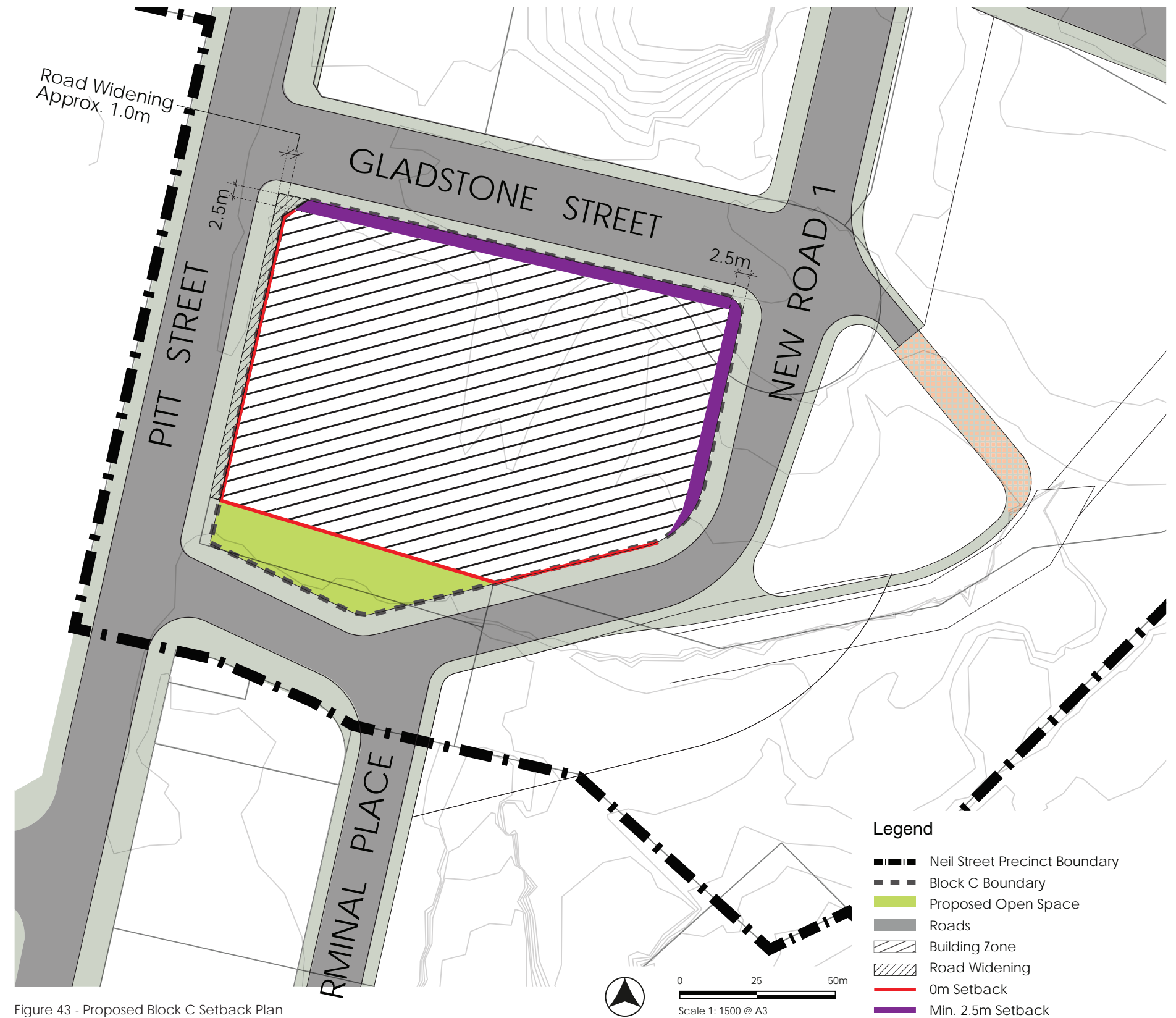


# 8 Site Specific Controls

## Controls

- Driveways and vehicular crossings are not permitted along Pitt Street
- Driveways and vehicular crossings are to be provided from New Road 1. Indicative locations are shown in Figure 38.

Building Height	
Along Pitt Street	• Max. 16 storeys (Refer Figure 38)
Terminal Place	• Max. 12 storeys (Refer Figure 38)
Gladstone Street	• Max. 8 storeys
Building Use	
B6 Zone - Corner of Gladstone Street and New Road 1	Ground Floor and above <ul style="list-style-type: none"> <li>• Residential</li> </ul>
B4 Zone - Along Pitt Street and Terminal Place	Ground and first floor <ul style="list-style-type: none"> <li>• Commercial/retail</li> </ul> Second floor and above <ul style="list-style-type: none"> <li>• Commercial / retail / residential</li> </ul>
B6 Zone New Road 1	Ground floor <ul style="list-style-type: none"> <li>• Commercial/retail</li> </ul> All floors above ground floor <ul style="list-style-type: none"> <li>• Commercial / residential</li> </ul>
Building Envelope Depth	
Commercial/retail and residential on all floors above podium	• Max. 22m
Setback	
Street setback	Pitt Street <ul style="list-style-type: none"> <li>• 0m</li> </ul>
	Gladstone Street <ul style="list-style-type: none"> <li>• Min. 2.5m</li> </ul>
	Terminal Place <ul style="list-style-type: none"> <li>• Min. 0m</li> </ul>
	New Road 1 <ul style="list-style-type: none"> <li>• Min. 2.5m</li> </ul>
Street Wall Height	
Along Pitt Street	• 3 storey podium with minimum height of 11m and maximum 14m.
Awning	
Along Pitt Street	• Min. 3m deep





# 8 Site Specific Controls

## 8.4 Block D

Block D is bounded by Neil Street to the north, the railway corridor (which runs northeast to southwest) to the east and south, Merrylands Train Station to the southwest and New Road 1 to the west with the overland flow path located to the east of New Road 1.

Although the accessibility of Block D is enhanced by the proposed Road 1, it is also constrained by the existing Sydney water culvert which runs through the site. The flood flow path which is envisioned to form part of the public open network provides an opportunity for Block D to enhance the public domain of the area by incorporating a centrally located space - Neil Street Park for recreational purposes.

Given the landscape setting, this Block is expected to accommodate secondary active uses (e.g. gymnasium, child care centre, corner shop, café) that support and enhance the liveability of the Precinct. The ground level activity within Block D will be focused along New Road 1.

The detailed, site specific controls within this section will define the scale and character of development at the Pitt and Neil Streets intersection, providing development that create a positive image.

The objectives are as follows:-

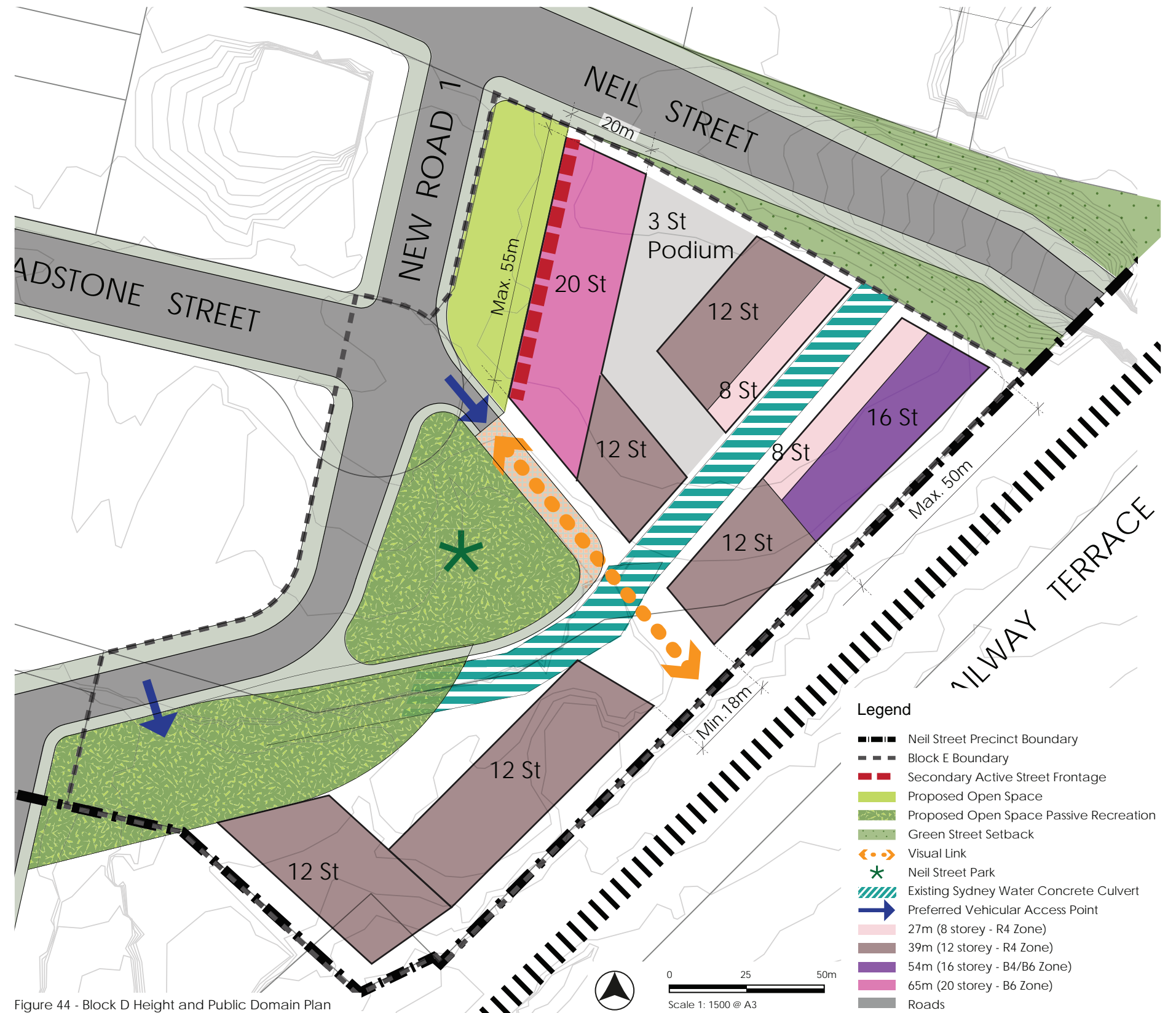
- To ensure the development contributes to the provision of public infrastructure.
- To ensure that the intersection of New Road 1 and Neil Streets is reinforced with greater height and create a distinct identity for the corner.
- To reinforce the open space through built form.
- To ensure scale and form of development contributes to the public domain and legibility of New Road 1 and Neil Street.

### 8.4.1 Site and Building Design

#### Public Domain

The key public domain features of this Block are:

- New Road 1 to the west
- Overland flow path and Neil Street Park to the west
- Neil Street to the north





# 8 Site Specific Controls

## Controls

- Secondary active frontage is to be provided where shown in Figure 44 (gymnasium, child care centre, corner shop, café).
- Secondary active frontage is to have a civic character, providing colonnades for the building at the intersection of Neil Street and New Road 1.

Refer to Section 5 for the future desired character of Neil Street Park .

### 8.4.2 Building Heights

Refined building heights are provided to determine the extent and location of height distribution within the Precinct.

## Controls

Development should comply with Block D Height Plan which indicates the maximum number of permissible storeys (Refer Figure 44 and 48).

### 8.4.3 Setbacks

To provide some flexibility in the configuration of buildings on site, building zones have been identified within which buildings can occur on the site. The building zone is determined by the street, side and rear setbacks.

The building zone cannot be totally taken up by buildings. The extent of the building zone that can be occupied by buildings is calculated by applying all the built form controls for the Precinct. The building configuration indicated in the diagrams is the preferred building configuration.

## Controls

- Provide setbacks as shown in Figure 47.

### 8.4.4 Public Domain Interface

Specific street frontage treatments are required to achieve consistency within and around the Precinct, and to reinforce the desired streetscape character. The streetscape character is determined by the design and consistency of the building edge, and the continuity of the built form interface relative to driveways and vehicular crossing.

## Controls

- Driveways and vehicular crossings are not permitted along Neil Street
- Driveways and vehicular crossings are to be provided from New Road 1. Indicative locations are shown in Figure 44.



Figure 45 - Interesting facades providing a visual entry to the Precinct



Figure 46 - London Renaissance. Taller Building providing visual reference



# 8 Site Specific Controls

Building Height	
Corner of Neil Street and New Road 1	• Max. 20 storeys (Refer Figure 47)
Corner of Neil Street and Railway Line	• Max. 16 storeys (Refer Figure 47)
All other buildings	• Max. 12 storeys (Refer Figure 47)
Building Use	
B6 Zone - Ground and first floor of 20 storey building	• Commercial/retail/residential
All other buildings + All floors above first floor of B6 Zone	• Residential
Building Envelope Depth	
All buildings except for the 20 storey tower	• Max. 22m
20 Storey Tower	• Max. 20m
Setback	
Street setback	From Neil Street <ul style="list-style-type: none"> <li>• Min. 2.5m</li> </ul>
Open Space setbacks	All other lots <ul style="list-style-type: none"> <li>• Comply with Figure 47</li> </ul>
Rear setbacks	From the Railway Corridor <ul style="list-style-type: none"> <li>• Min. 6m (Min. 3m in the southern corner)</li> </ul> From Merrylands Transit Interchange <ul style="list-style-type: none"> <li>• Min. 6m</li> </ul>
Awning	
Along Boulevard Park	• Min. 3m deep



# 8 Site Specific Controls



Figure 48 - Section C1 - C1



# 8 Site Specific Controls



Figure 49 - Public domain character - footpath with awning



Figure 51 - Public open space supporting the needs of the active street frontage



Figure 52 - Primary active street frontage



Figure 50 - Building separation providing visual relief and minimise the impact of built form



Figure 53 - Shared Zone - Pedestrian link around Neil Street Park



# 8 Site Specific Controls

## 8.5 Block E

Block E is bounded by the old brickworks site and the Holroyd Gardens to the north, the railway corridor (which runs northeast to southwest) to the east and southwest, Neil Street to the south and Block F (13-15 Neil Street) to the west.

The objectives are as follows:-

- To ensure the development contributes to the provision of public infrastructure.
- To ensure scale and form of development contributes to the public domain and is sympathetic to the residential development to the north.

### 8.5.1 Site and Building Design

#### Public Domain

The key public domain features of this Block are:

- New Road 2
- Neil Street to the south
- Overland flow path

#### Controls

- Proposed built form should reinforce and address the overland flow path.

### 8.5.2 Building Heights

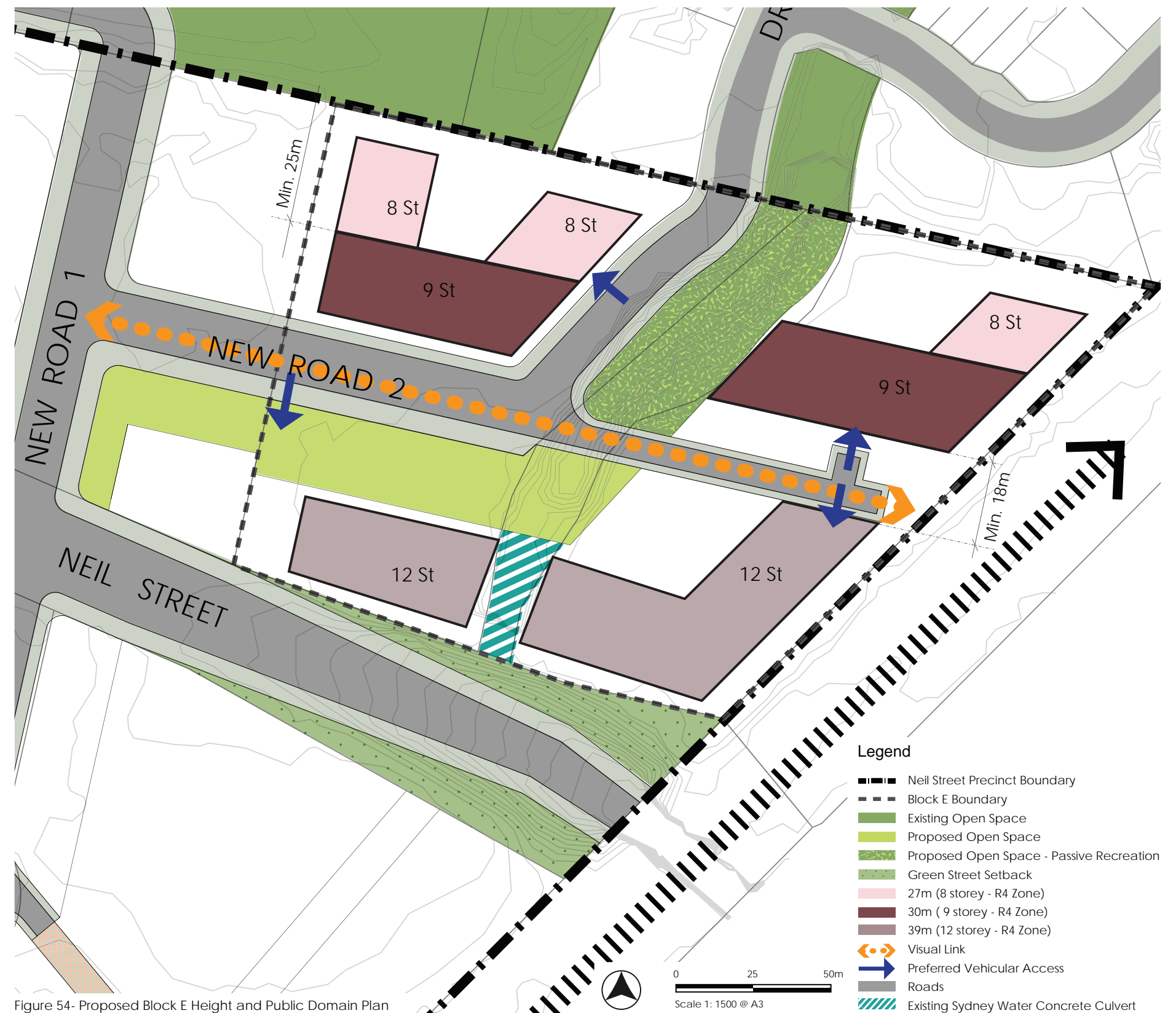
#### Controls

- Development should comply with Block A Height Plan which indicates the maximum number of permissible storeys (Refer Figure 54).

### 8.5.3 Setbacks

#### Controls

- Provide setbacks as shown in Figure 55.





# 8 Site Specific Controls

## 8.5.4 Public Domain Interface

### Controls

- Driveways and vehicular crossings are not permitted along Neil Street
- Driveways and vehicular crossings are to be provided from New Road 2. Indicative locations are shown in Figure 54.
- Provide a landscape setback along Neil Street and New Road 2 in accordance with Figures 54 and 55.

Building Height	
Building along the northern boundary	• Max. 8 storeys (Refer Figure 54)
Parts of buildings north of New Road 2	• Max. 9 storeys (Refer Figure 54)
Along Neil Street and the railway corridor	• Max. 12 storeys (Refer Figure 54)
Building Use	
R4 Zone - All floors	• Residential
Building Envelope Depth	
All buildings	• Max. 22m
Setback	
Street setback	North and West of New Road 2
	• Min. 2.5m
Other setbacks	From Neil Street
	• Min. 2.5m
Other setbacks	From the boundary parallel to the railway line
	• Min. 6m
	From western boundary
	• Min. 12m (south of New Road 2 - comply with minimum separation controls)
	• Min. 6m (north of New Road 2)
	From Holroyd Gardens to the north
	• Min. 6m
	On other lots
	• Min. 9m
Other setbacks	From the southern boundary of overland flow path
	• Min. 2.5m
	Eastern boundary of overland flow path
	• Min. 12m and 0m

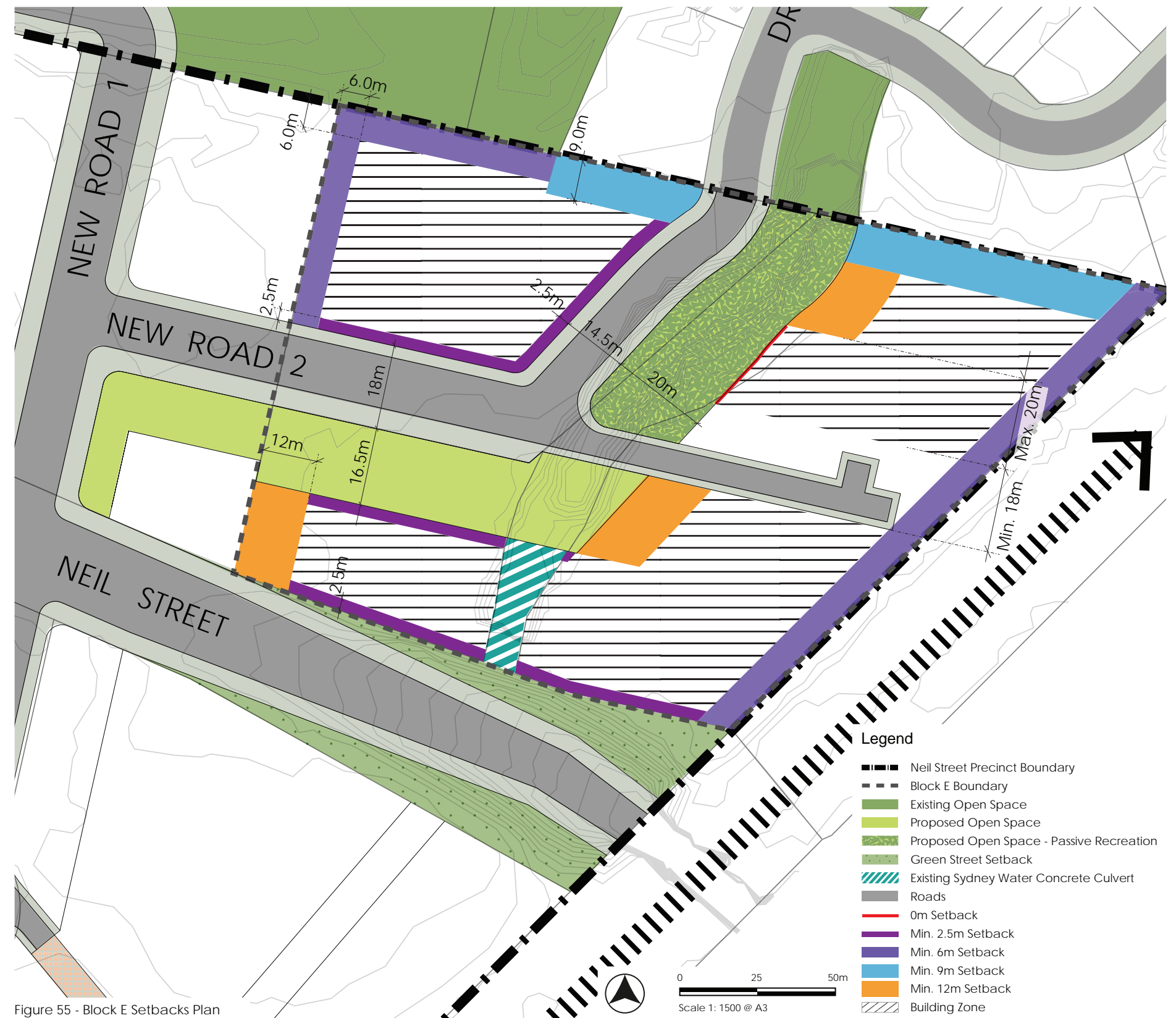


Figure 55 - Block E Setbacks Plan

# 8 Site Specific Controls

## 8.6 Block F

Block F is bounded by the Holroyd Gardens to the north, Block E to the east, Neil Street to the south and Block A to the west. The New Road 1 and New Road 2 form a 'T' intersection within Block E.

The accessibility of Block F although is enhanced by the proposed New Road 1 and New Road 2, it also divides the site into 3 lots impacting on its development potential and functionality. A potential mid-block connection, an extension of New Road 2, will enhance pedestrian permeability within the Precinct and with the surrounding development.

The objectives are as follows:-

- To ensure the development contributes to the provision of public infrastructure.
- To ensure that the intersection of Neil Street and New Road 1 create a quality identity for the corner.

### 8.6.1 Site and Building Design

#### Public Domain

The key public domain features of this Block are:

- New Road 1
- New Road 2
- Neil Street to the south
- Overland flow path

### 8.6.2 Building Heights

#### Controls

- Development should comply with Block F Height Plan (Refer Figure 56).

### 8.6.3 Setbacks

#### Controls

- Provide setbacks as shown in Figure 57.

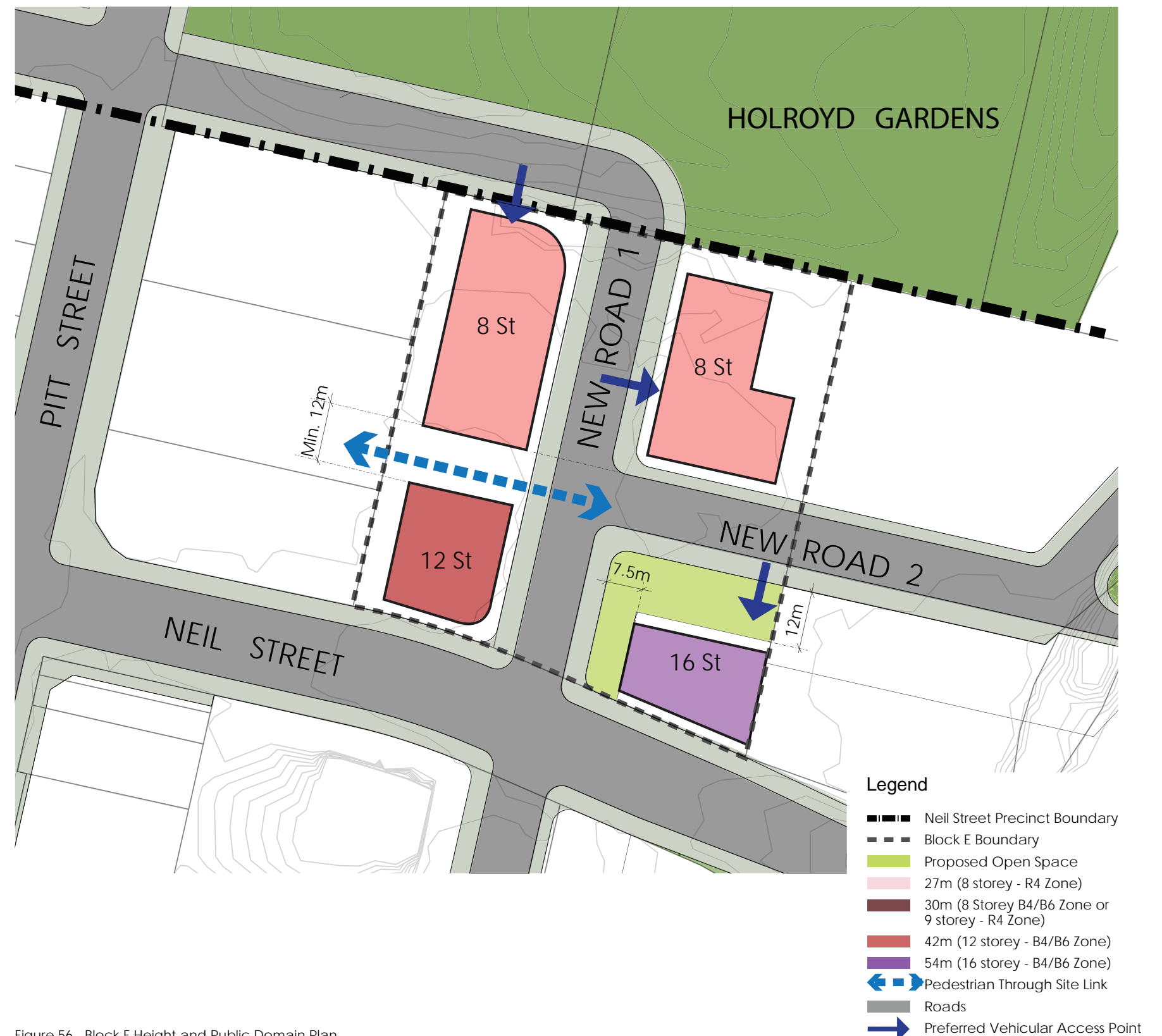


Figure 56 - Block F Height and Public Domain Plan



# 8 Site Specific Controls

## 8.6.4 Public Domain Interface

### Controls

- Driveways and vehicular crossings are not permitted along Neil Street.
- Driveways and vehicular crossings are to be provided from New Road 1 and New Road 2. Indicative locations are shown in Figure 56.

Building Height	
North of New Road 2	• Max. 8 storeys (Refer Figure 56)
Northwest corner of Neil Street and New Road 1	• Max. 12 storeys (Refer Figure 56)
Northeast corner of Neil Street and New Road 1	• Max. 16 storeys (Refer Figure 56)
Building Use	
B6 Zone - Ground Floor of 12 and 16 Storey Building	• Commercial/retail/residential
All floors above First Floor	• Residential
All other buildings	• Residential
Building Envelope Depth	
All buildings	• Max. 22m
Setback	
Street setback	New Road 1 (North of New Road 2)
	• Min. 2.5m
	From New Road 2 (North)
	• Min. 2.5m
	From the southern boundary of public open space
Other setback	• Min. 2.5m
	From Neil Street
	• Min. 2.5m
	From Sheffield Street Extension
	• Min. 2.5m
Other setback	From Holroyd Gardens to the north
	• Min. 6m
	From the eastern boundary
	• Min. 6m

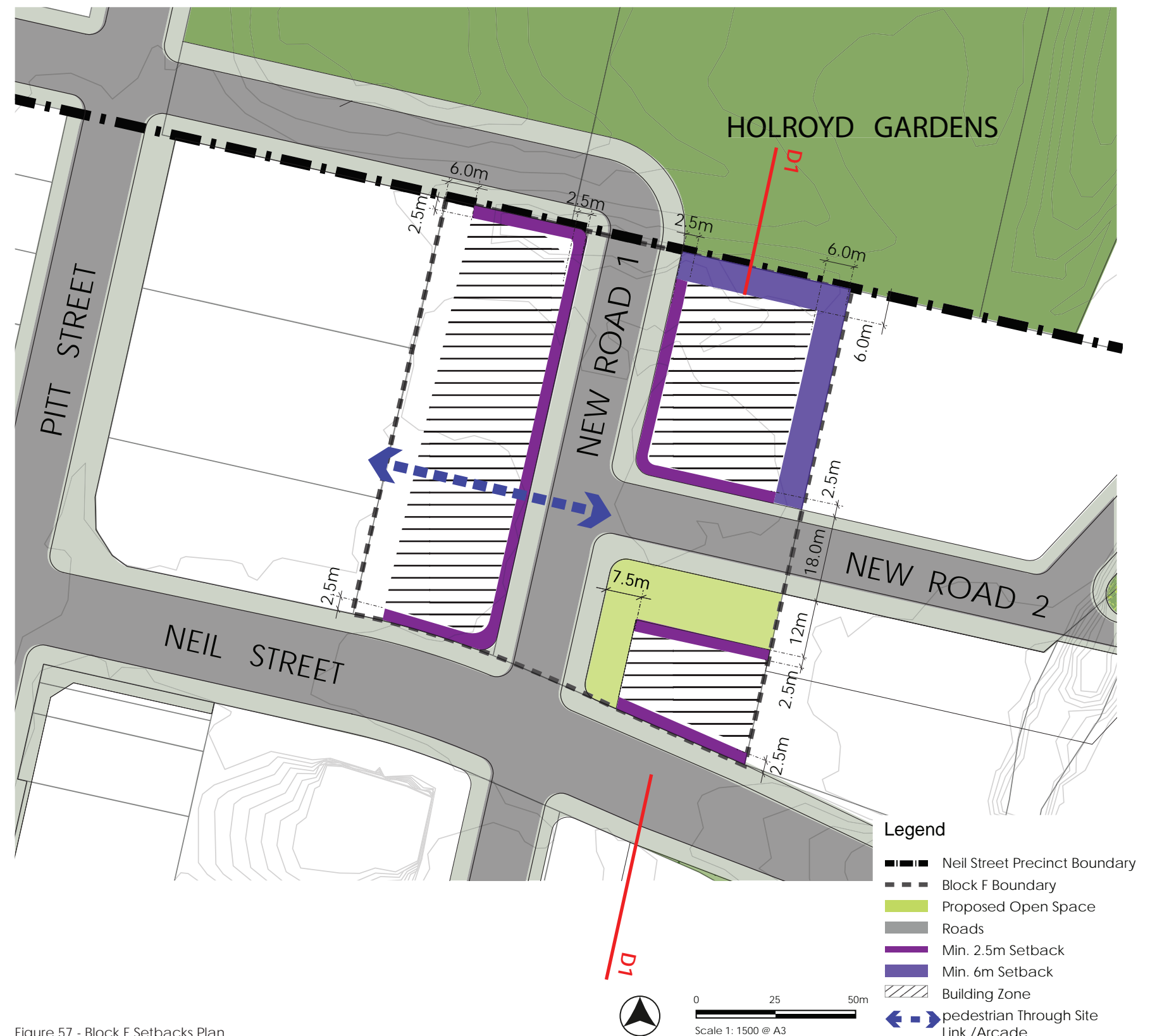


Figure 57 - Block F Setbacks Plan

# 8 Site Specific Controls

## 8.7 Block G

Block G is bounded by Sheffield Street to the north, Pitt Street to the east, the Stockland Mall to the south and residential development to the west.

The objectives are as follows:-

- To provide a range of uses supporting the predominantly commercial use within the Merrylands Centre, and generating activity at ground level.
- To ensure that the intersection of Neil Street and Pitt Street create a quality identity for the corner.

### 8.7.1 Site and Building Design

#### Public Domain

The key public domain features of this Block are:

- Pitt Street
- Neil Street

#### Controls

- Primary active frontages are to be provided where shown in Figure 58.
- Primary active frontage are to have a civic character, providing an awning along the edge of Pitt Street.

### 8.7.2 Building Heights

#### Controls

- Development should comply with Block F Height Plan (Refer Figure 57 and 60).

### 8.7.3 Setbacks

#### Controls

- Provide setbacks as shown in Figure 59.

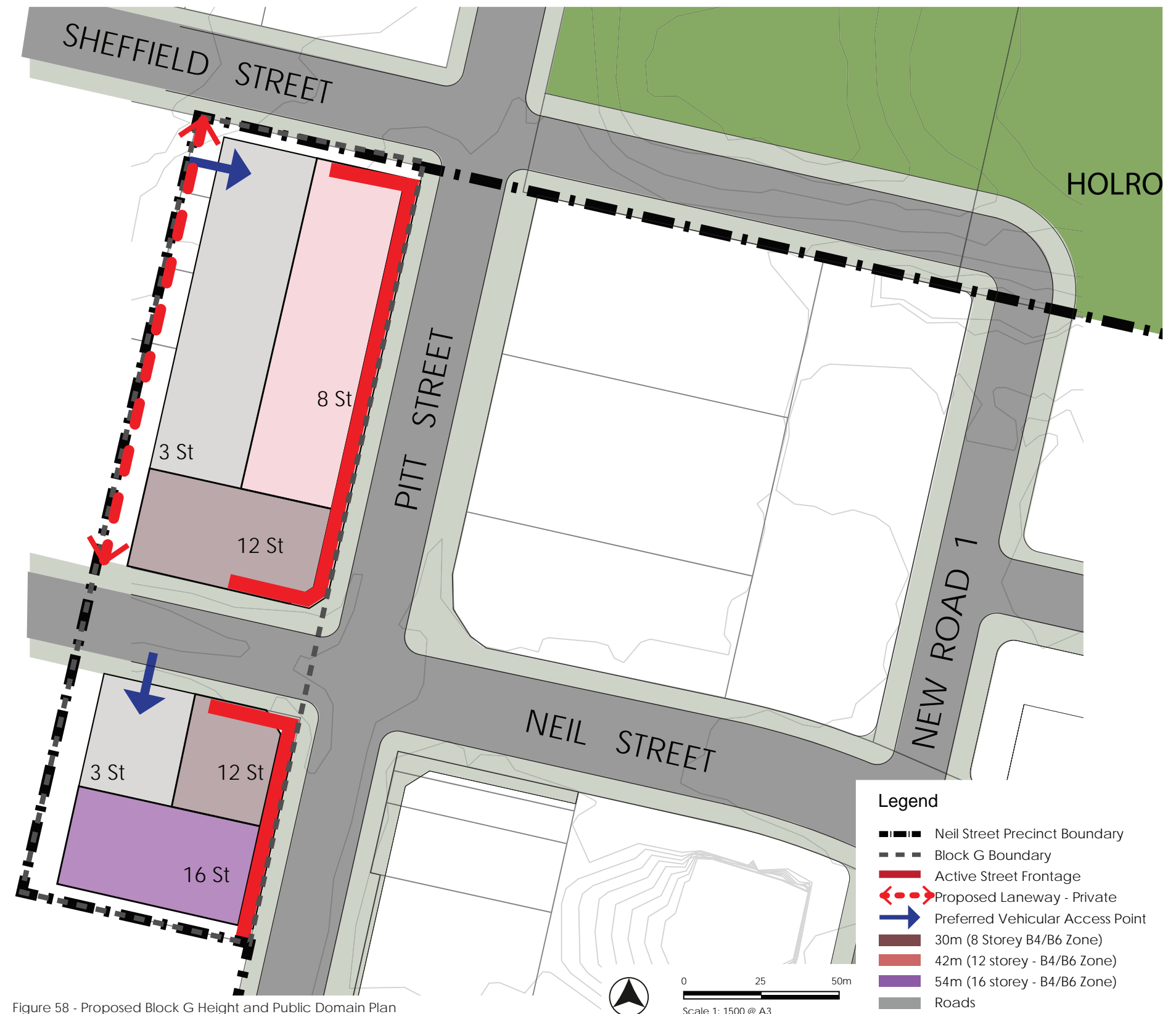


Figure 58 - Proposed Block G Height and Public Domain Plan



# 8 Site Specific Controls

## 8.7.4 Public Domain Interface

### Controls

- Driveways and vehicular crossings are not permitted along Pitt Street
- Driveways and vehicular crossings are to be provided from Sheffield Street and Neil Street. Indicative locations are shown in Figure 57.

Building Height	
B4 Zone - Northwest and southwest corner of Neil Street and Pitt Street	• Max. 12 storeys (Refer Figure 59)
B4 Zone - Buildings along Pitt Street	• Max. 16 storeys (Refer Figure 59)
B4 Zone - Other buildings north of Neil Street	• Max. 8 storeys (Refer Figure 59)
Building Use	
All buildings	Ground and first floor
	• Commercial / retail
	First floor and above
	• Residential / commercial
Building Envelope Depth	
All buildings	• Max. 22m
Setback	
Street setback	From Pitt Street
	• 0m
	From Neil Street
	• 0m
	From Sheffield Street
	• Min. 3.0m
Rear setback - North of Neil Street	From the western boundary
	• Min. 8.0m
Rear setback - South of Neil Street	From the western boundary
	• Min. 6.0m
Side setback - South of Neil Street	From the southern boundary
	• Min. 3.0m
Street Wall Height	
Along Pitt Street	• 3 storey podium with minimum height of 11m and maximum 14m.
Awning	
Along Pitt Street	• Min. 3m deep

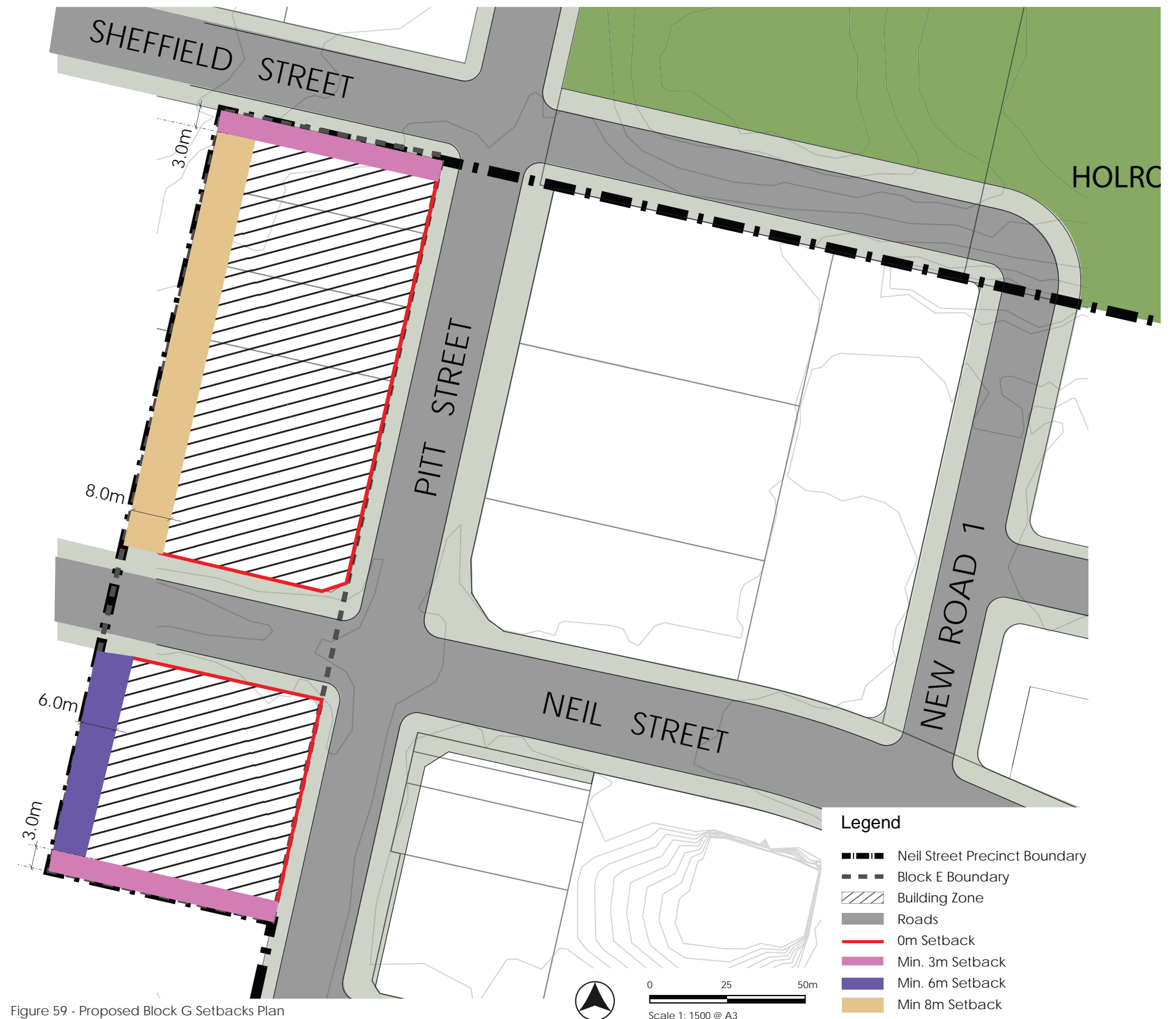


Figure 59 - Proposed Block G Setbacks Plan

# 8 Site Specific Controls



Figure 60 - Section D1 - D1